

Agenda

Local Traffic Committee Meeting **14 February 2024**

A Local Traffic Committee Meeting will be held in the Ballina Shire Committee Room, 40 Cherry Street, Ballina on **14 February 2024 commencing at 10:00 am.**

1. Attendance & Apologies
2. Minutes of Previous Meeting
3. Deputations by Members of Public or Councillors
4. Summary Report – Recent Decisions of Council in Response to LTC Recommendations
5. Items to be Referred to Council
6. Items to be Referred to the General Manager's Delegate
7. Items for Traffic Engineering Advice
8. Information of the Committee
9. Regulatory Matters on Classified Roads (GM's Delegate)
10. Items Without Notice
11. Next Meeting

John Truman
Director
Civil Services Division

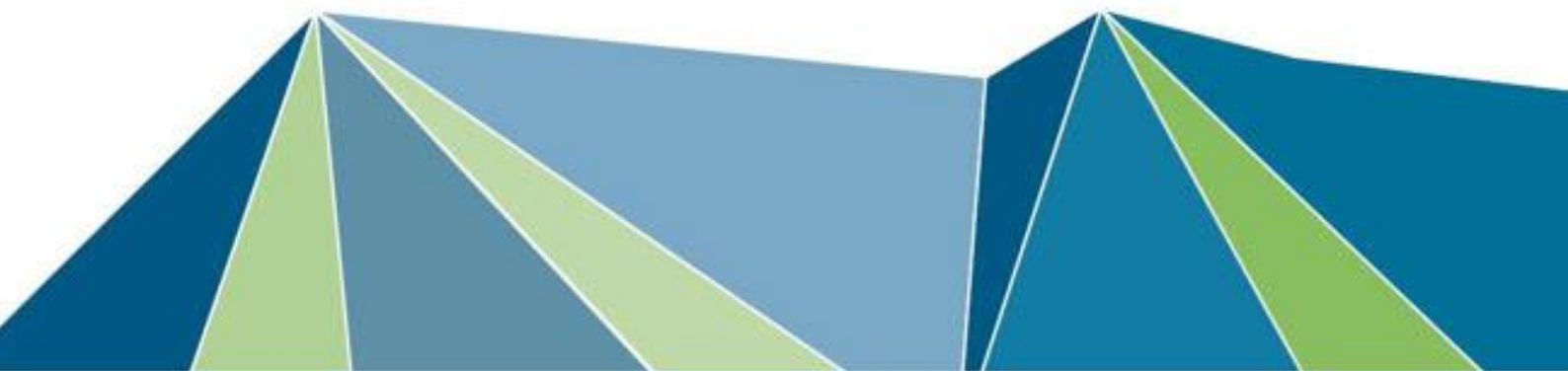


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1. Attendance & Apologies
 2. Minutes of Previous Meeting
 3. Deputations by Members of Public or Councillors
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1. Attendance & Apologies

2. Minutes of Previous Meeting

A copy of the Minutes of the Local Traffic Committee Meeting held on Wednesday 13 December 2023 were distributed with the agenda.

RECOMMENDATION

That the Committee confirms the Minutes of the Local Traffic Committee Meeting held on Wednesday 13 December 2023.

3. Deputations by Members of Public or Councillors

4. Summary Report - Recent Decisions of Council in Response to LTC Recommendations

Nil Items

5. Items to be Referred to Council

5. Items to be Referred to Council

Nil Items

6.1 Proposed Electric Vehicle Chargers on Public Streets

6. Items Referred to General Manager's Delegate

6.1 Proposed Electric Vehicle Chargers on Public Streets

Introduction

Through a commercial trial between EVX and Essential Energy (EE), Council has been offered the opportunity to install two pole mounted electric vehicle (EV) chargers at two different sites within the Shire.

Information

In 2023 the Northern Rivers Joint Organisation of Council's (NRJO) participated in a site analysis study to identify suitable sites for installation of pole mounted EV chargers through a trial partnership program with EVX (the supplier) and Essential Energy.

The purpose of the trial is to gather data to determine the point at which the pole mounted EV infrastructure becomes viable (margins support operating costs), estimated in 2026. Essential Energy is providing abatements on costs during the rollout phase from 2023 - 2026 to incentivise the more rapid rollout of this important community infrastructure.

These are level 2 chargers, so the slower type, but are ideal to have in locations such as carparks or Council facilities that enable the public to charge whilst stopping for a couple of hours.

EVX conducted site audits which considered:

- the capacity of the Essential Energy infrastructure (poles),
- space,
- parking permit conditions,
- and proximity and access to shops.

The study revealed four preferred locations. The top four sites are:

1. 44 Cherry Street, Ballina
2. 1 Ross Street, Lennox Head
3. 2 Grandview Street, East Ballina
4. 18 Shelley Beach Road

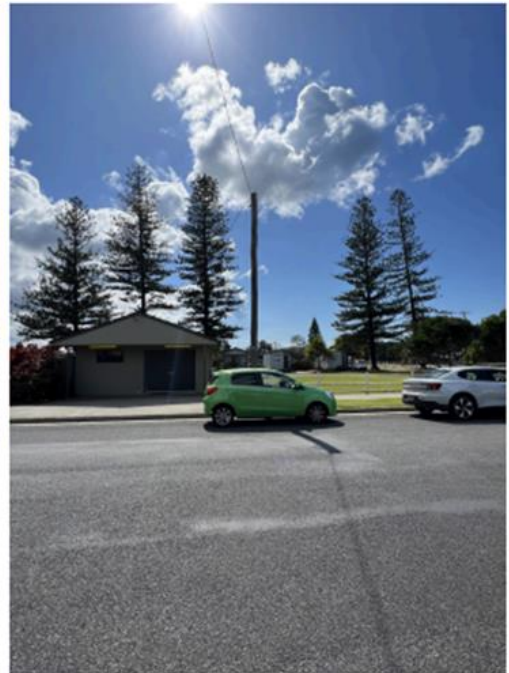
The Cherry Street, Ballina and Ross Street, Lennox Head sites are currently favoured for the EV chargers.

It is understood that EVs using the chargers need to supply their own cables that are long enough to accommodate charging when the car charging point is at the far end of the vehicle (in relation to the pole mounted charger).

6.1 Proposed Electric Vehicle Chargers on Public Streets



Site 1: 44 Cherry Street – close to Art Gallery, shops, cafes.



Site 2: 1 Ross Street, Lennox Head – close to beach, lake, surf club

Proposed Line Marking and Signage

Cherry Street, Ballina Location

This location is on the west side of Cherry Street between Holden Lane and Crane Street (near the Art Gallery). The area is currently within a 18m long zone, 45 degree Angle Parking – Rear to Kerb Vehicles under 6m Only. The parking bays are not currently linemarked. The northern end adjoins a disabled car parking zone. The proposed EV charging area would occupy two parking spaces immediately south of the disabled car parking zone.

AS2890.5 recommends a 3.5m for 45 degree parking bay width which would accommodate 5 vehicles in the 18m length. The width of Cherry Street is 11m from kerb to centreline and more than adequate to accommodate AS standard compliant 60 degree parking. If 60 degrees is used, the parking bay width reduces to 2.9m and 6 vehicles could be accommodated in the 18m length. Observations also confirm that drivers resist parking at 45 degrees and when bays are unmarked, nearly always park at a greater angle.

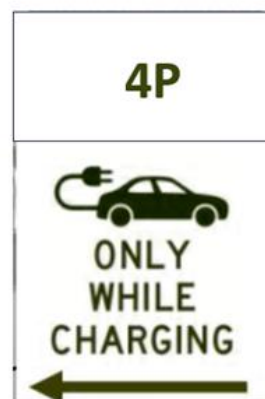
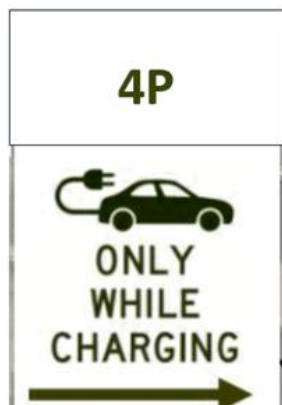


Cherry Street Aerial View

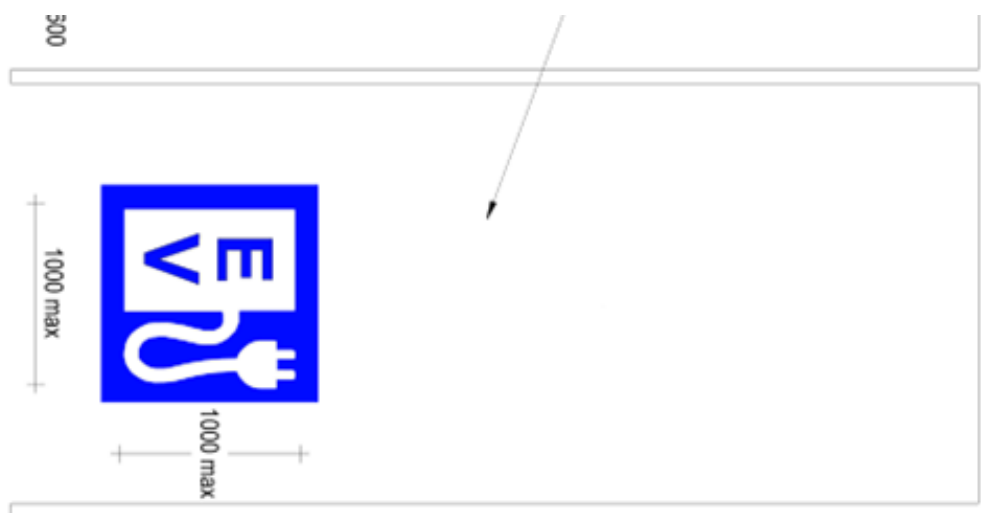
It is proposed to:

- Sign and linemark the west side of Cherry Street between Holden Lane and Crane Street for 60 degree angle parking, bay width 2.9m measured along kerb.
- Retain the disabled car parking area to the north in its current location but, linemarked at a 60 degree angle.
- Provide additional signage and pavement markings (see examples below) for the northern 2 bays (adjacent to the disabled parking bay) to designate as a 4P EV charging zone.

6.1 Proposed Electric Vehicle Chargers on Public Streets



Proposed Signage



Proposed Pavement Markings

6.1 Proposed Electric Vehicle Chargers on Public Streets

Ross Street, Lennox Head Location

This location is on the north side of Ross Street centred on a pole 42m west of Pacific Parade. This is a kerbside parallel parking area, adjacent to the caravan park, with no time restrictions or parking bay linemarking.



Ross Street Aerial View

It is proposed to:

- Sign and linemark two kerbside parallel parking bays on the north side of Ross Street Lennox Head, centred on an electricity pole 42 m west of Pacific Parade.
- Provide additional signage and pavement markings (see previous examples) for the marked parallel parking bays to designate as a 4P EV charging zone.

RECOMMENDATIONS

That the Committee support:

1. Changing the existing, 18m long “45° Angle Parking – Rear to Kerb, Vehicles under 6m Only” zone on the west side of Cherry Street Ballina, north of Holden Lane to “60° Angle Parking – Rear to Kerb, Vehicles under 6m Only”.
2. Retaining the existing disabled parking bay at the northern end of 1 above but, changing to 60 degrees angle.
3. Designating the two northern parking bays of 1 above as 4P EV charging spaces
4. Designating two kerbside parallel parking bays on the north side of Ross Street Lennox Head, centred on an electricity pole 42m west of Pacific Parade as 4P EV charging spaces.

5. Provision of associated signage, pavement markings and parking bay linemarking for 1, 2, 3 and 4 above, as generally described in the body of this report.

Attachment(s)

Nil

6.2 Proposed Pedestrian and Road Safety Improvements, Main Street and The Avenue, Alstonville

6.2 Proposed Pedestrian and Road Safety Improvements, Main Street and The Avenue, Alstonville

Introduction

Council has funding under the Get NSW Active 2023/2024 program for safety upgrades at the Main Street/The Avenue intersection at Alstonville. The Committee's formal support is requested for these works including associated regulatory signage and markings and provision of a new pedestrian crossing.

Information

This project is located at the intersection of Main Street and The Avenue, Alstonville NSW including Main Street from Bugden Avenue to 20m West of The Avenue, and The Avenue from Main Street to Daley Street. The locality includes three schools, a church, an op shop, and at least one health facility with a second health facility approved through the development application process. The design includes median extension, raised safety platform, new pedestrian crossing, new accessible car park, stormwater upgrades and associated adjustments to the adjoining driveways and shared paths.

Attached to this report are "Engineering Report, The Avenue & Main Street, Alstonville Get NSW Active 2023/24" V1 January 2024 and design plans of the project. This report analyses the site, details proposed works and provides justification of the proposed new pedestrian crossing in accordance with Transport for NSW "Pedestrian Crossing Guideline" (TS 00043:1:0, 13 September 2022).

RECOMMENDATIONS

That the Committee support the proposed road and pedestrian safety improvements, associated regulatory signs and pavement markings, and additional marked pedestrian crossing as detailed on page 11, "*Figure 5: Regulatory Signage and line marking included in the proposal*" of the attachment to this report.

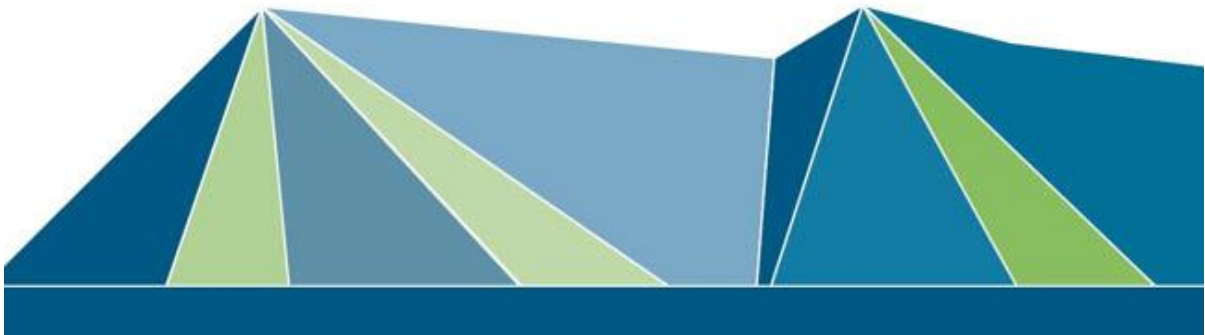
Attachment(s)

1. Main Street and The Avenue Alstonville Engineering Report [↓](#)
2. Main Street and The Avenue Alstonville Design Plans [↓](#)



Engineering Report The Avenue & Main Street, Alstonville Get NSW Active 2023/24

V1 January 2024



Engineering Report The Avenue & Main Street, Alstonville

Version	Date	Reason/Comments	Name/Position
1.0	January 2024	Original Issue	Fred Moss Design Engineer



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Executive Summary:

This project is located at the intersection of Main Street and The Avenue, Alstonville NSW including Main Street from Bugden Avenue to 20m West of The Avenue, and The Avenue from Main Street to Daley Street. This locality includes three schools, a church, an op shop, and at least one health facility with a second health facility approved through the development application process.

Main Street and the Avenue are road assets of Ballina Shire Council and are not classified roads.

This project is funded under the Get NSW Active 2023 / 2024 program. The funding requires shared path and cycling upgrades including pedestrian priority at road crossings.

A primary purpose of this report, beyond documenting the design process, is to detail critical elements valuable to the Local Traffic Committee in accepting the installation of the proposed new pedestrian crossing. Key supporting documentation is addressed including the Transport for NSW (TfNSW) Pedestrian Crossing Guideline (13 September 2022) and TfNSW Supplement to Australian Standard AS 1742.10-2009(16 March 2021).

Further, the design plan recognises the various pedestrian desire lines at the locality and existing issues in relation to vehicle movements through this part of Alstonville. Further upgrades are implemented to improve vehicles manoeuvring at the intersection.

The design includes median extension, raised safety platform, new pedestrian crossing, new accessible car park, stormwater upgrades and associated adjustments to the adjoining driveways and shared paths.

This project directly relates to Council's Alstonville Strategic Plan and furthers the Council Pedestrian and Mobility Plan.

The TfNSW Supplement to Australian Standard AS 1742.10-2009 warrants for the implementation of a pedestrian crossing of over 20 pedestrians for two one hour periods per day with vehicle volume exceeding 200 are met. Counts from 2011 show that the criteria has been met for some time. Known development and alignment to strategic documentation and the implementation of the pedestrian upgrades is expected to increase active transport in increasing these numbers.

The second TfNSW Supplement criteria of the multiplication of Pedestrian times Vehicle is almost met at 50 000 not quite to the required 60 000, which the LTC is recommended to accept.

The current TfNSW Pedestrian Crossing Guideline warrants are readily met.

A formalised crossing consistent to the scope of the funding source is recommended.

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Background:

Project Inception

Project Site:

This project is located at the intersection of Main Street and The Avenue, Alstonville NSW, including:

Main Street from Bugden Avenue to 20m West of The Avenue, and

The Avenue from Main Street to Daley Street.

Main Street and the Avenue are road assets of Ballina Shire Council and are not classified roads.



Figure 1: Project Locality Plan

Funding:

This project is funded under the Get NSW Active 2023 / 2024 program. The funding requires shared path and cycling upgrades including pedestrian priority at road crossings. The funding scope is quoted in Appendix I.

The project budget is 100% funded under this external source to the value of \$674 500 .00

6.2 Proposed Pedestrian and Road Safety Improvements, Main Street and The Avenue, Alstonville

Ballina Shire Council

Purpose

This report: Compiles the key engineering and social factors relating to this locality, documents the design process in developing the project construction plan, and detail critical elements valuable to the Local Traffic Committee (LTC) in accepting the installation of the proposed new pedestrian crossing.

Key supporting documentation is addressed including:

The Transport for NSW (TfNSW) Pedestrian Crossing Guideline (13 September 2022), and

TfNSW Supplement to Australian Standard AS 1742.10-2009(16 March 2021).

Locality Appreciation

Key properties in the vicinity of this project site are stated in Table 1 with distances to site as straight-line distances to the center of the intersection of Main Street and The Avenue.

Table 1: Key trip generators near the project site

Property Address	Property Name	Distance to site
58 Main Street	Alstonville Primary School	0m
21 Cawley Close	Alstonville High School	200m West
11 Perry Street	St Joseph's Primary School	190m North West
61 Main Street	Alstonville Clinic	0m
59 Main Street	DA Approved Health Facility	0m
1 The Avenue	Anglican Op Shop	70m South West
60 Main Street	Uniting Church	0m
6-8 The Avenue	Anglican Church	100m South West
15 The Avenue	Maranoa Aged Care	210m South West
Main Street, East of Bugden Avenue	Alstonville Town Centre	90m South East
22-40 Commercial Road	Alstonville Showgrounds	210m East
2 Perry Street	Alstonville Police	250m North West

6.2 Proposed Pedestrian and Road Safety Improvements, Main Street and The Avenue, Alstonville

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Pedestrian desire lines through the project site are shown in Figure 2.



Figure 2: Pedestrian Desire Lines

6.2 Proposed Pedestrian and Road Safety Improvements, Main Street and The Avenue, Alstonville

Ballina Shire Council

Existing Site Conditions

The site includes the following regulatory signage:

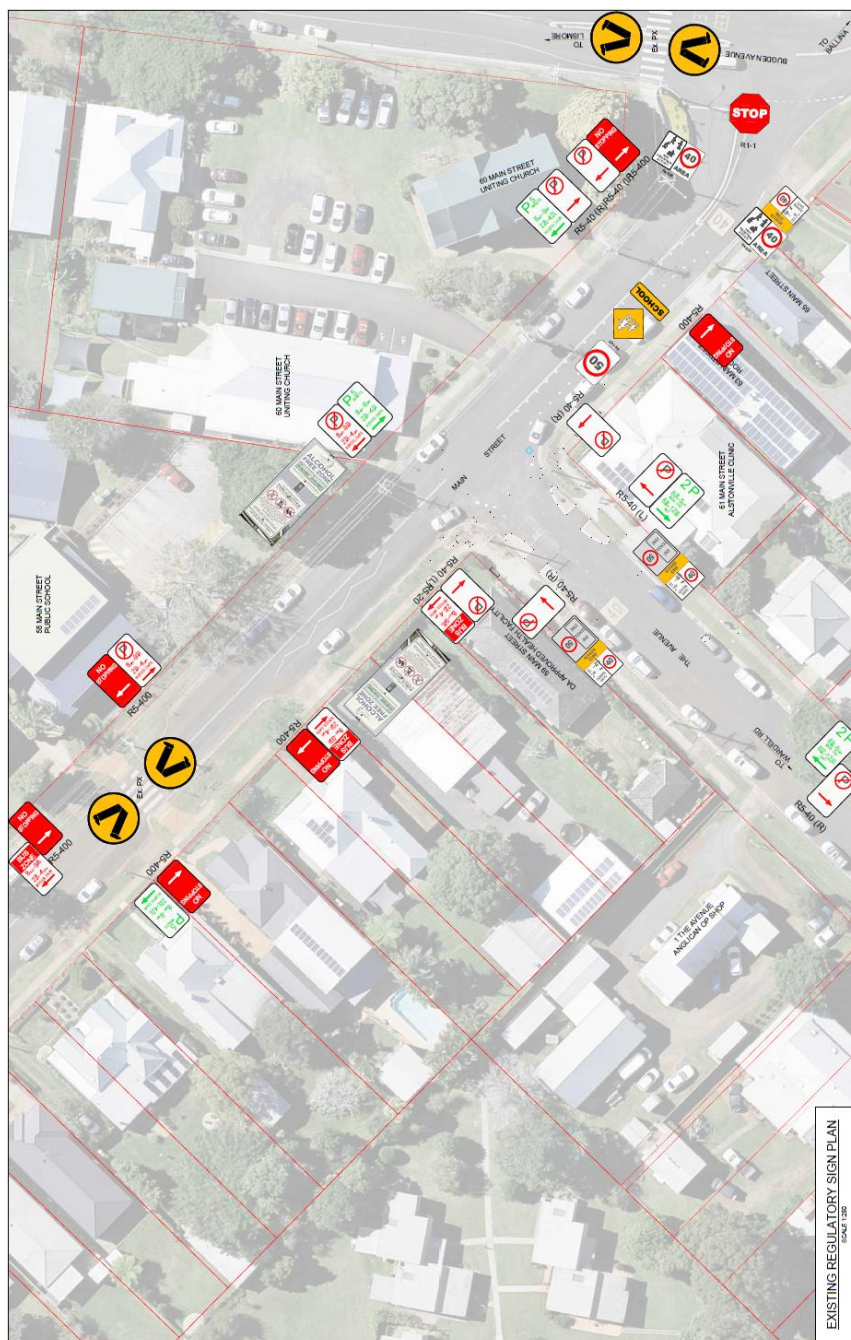


Figure 3: Existing Regulatory Signage

6.2 Proposed Pedestrian and Road Safety Improvements, Main Street and The Avenue, Alstonville

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Main Street Alstonville includes a shared path on both sides of the road.

The southern side path includes kerb blisters to narrow the width where pedestrians must cross The Avenue and to control parking adjacent to the crossing location.

The Avenue includes foot paths on both sides of the road.

Notably, there are few formal locations for pedestrians and cyclists to cross Main Street in the North-South direction. Two pedestrian crossings exist for this crossing direction: Mid-block near Bugden Lane in the town centre, 127m East of Bugden Avenue, and Mid-block fronting Alstonville Public School, 60m West of The Avenue.

Pedestrian road crossing locations are sound in the East-West direction, including:

One formal pedestrian crossing at the Intersection of Bugden Avenue and Main Street on the North Side of Main Street,

Kerb Extensions at informal crossing point at the Intersection of Main Street and the Avenue, on the South Side of Main Street, and

Kerb Extension and one way configuration of Bugden lane at informal crossing point at the intersection of Bugden Lane and Main Street on the North side of Main Street.

Main Street is typically 10m wide kerb to kerb with varying one way and two way crossfalls from 2% to 6%.

The Avenue is typically 10m wide kerb to kerb with two way crossfall from 1 to 4%

The roads within the project site are frequented twice daily with school buses but is not on any timetabled bus route, which use Bugden Avenue and Main Street East of Bugden Avenue. On Main Street however, during morning and afternoon school zone times, bus 661 between Lismore and Ballina was observed at the Alstonville Primary School. The [Network map of buses](#) is extracted in Figure 4:

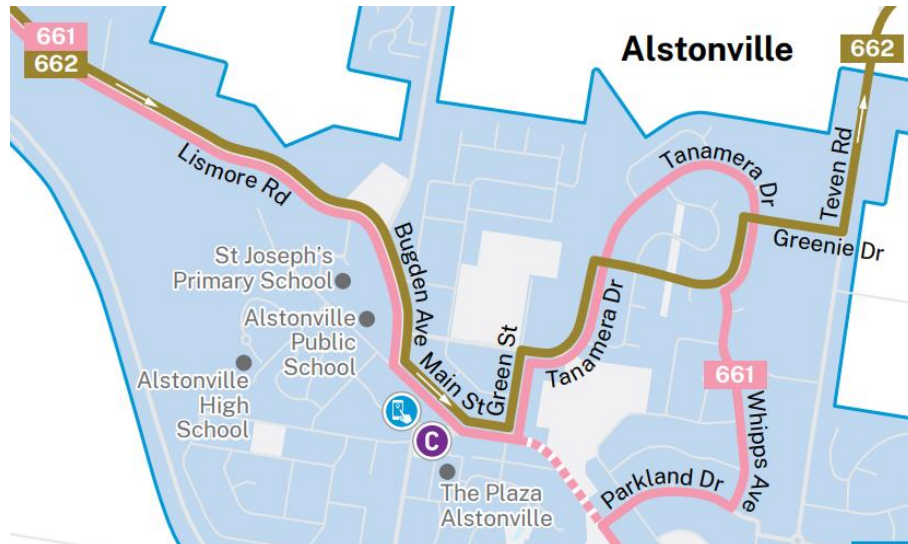


Figure 4: TfNSW Bus Network Map

Ballina Shire Council

Proposal:

The proposal considers the following documents in its development:

Council's Alstonville Strategic Plan Section 3, Item 12

Council Pedestrian and Mobility Plan

Council Bike Plan

Northern Rivers Joint Organisation Strategic Plan 2022-24 Strategic Action 6.3 Dot Point 2

The design plan recognises the various pedestrian desire lines at the locality and existing issues in relation to vehicle movements through this part of Alstonville. Further upgrades are implemented to improve vehicles manoeuvring at the intersection.

The design includes median extension, raised safety platform, new pedestrian crossing, new accessible car park, stormwater upgrades and associated adjustments to the adjoining driveways and shared paths.

Shared Path

Existing foot and shared paths varied from 0.9 to 2.0m wide.

No works are proposed on the 1.5m wide footpath on the northern side of Main Street which was upgraded from the previous 1.2m wide footpath since 2018. This footpath however is in Council's PAMP and Bike Plan as a shared path (Min 2.0m wide) and will need further upgrade in the future.

The priority in this project is to widen the narrow southern footpath and upgrade it into a shared path instead. The proposal consistently provides 2.0 to 2.5m shared paths. This creates a consistent shared path network between the Alstonville Town Centre and Schools Precinct.

Main Street & The Avenue intersection

Key Project Items:

A Raised Safety Platform (RSP) is included at the intersection of Main Street and The Avenue to slow vehicles on all legs. At the pedestrian crossing location this results in a flush level surface with no trip hazards for pedestrians. The road area is retained as bitumen to delineate from the concrete shared paths. The pedestrian crossing is warranted, below.

The Concrete Splitter Median at the intersection of Bugden Avenue with Main Street is extended down Main Street at 1.2m wide to the RSP. Benefits include:

- RSP Eastbound departure ramp flatter (optimised for buses)
- Enforces 12.5m design vehicle to take better swept paths through the intersection (bus)
- Narrows the available Main street road area to deter u-turns and unsafe car manoeuvres with drivers deviating from travel lanes towards parked positions
- Does not impact the adjoining church driveways based on a 9m design vehicle (Hearse).

Changes to parking creates 1 additional accessible park and consolidates the various controlled parking treatments. This is detailed below the warrants for pedestrian crossings.

6.2 Proposed Pedestrian and Road Safety Improvements, Main Street and The Avenue, Alstonville

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Pedestrian Crossings:

AS1742.10-2009 Provides primary guidance on the implementation of Pedestrian crossings. It is supplemented by the TfNSW supplement to AS1742.10. Both these key documents are addressed in warranting the pedestrian crossing with direct relation to the TfNSW Pedestrian Crossing Guideline.

Table 2 addresses AS1742.10-2009 Requirements including council comments.

Table 2: AS1742.10-2009 Requirements

AS1742.10-2009 Item	Comment
No more than one lane of moving traffic in any one direction	Condition met, two-way two lane road.
Adequate Sight Distance	Vehicles in The Avenue meet Austroads Minimum Gap Sight Distance Vehicles in Main Street meet Safe Intersection Sight Distance Kerb Extensions are applied.
Speed Limit ≤50km/h & 85 th ile speed < 60kmh	Design Speed 25km/h on RSP Current 85 th ile speeds are OK: Main Street Eastbound: 36.4km/h Main Street Westbound: 32.6km/h The Avenue Northbound: 39.6km/h The Avenue Southbound: 37.3km/h
Shall not be used on arterial roads	Condition met, non-arterial road

TfNSW Supplement to AS1742.10 (March 2021) section 6.3 onwards provides Additional Information and Departure (Transport Process) Commentary addressed in Table 3.

Table 3: TfNSW Supplement to AS1742.10 Requirements

TfNSW Supplement to AS1742.10	Comment
Refer supplementary document: TfNSW Delineation Guide – Section 7 Transverse Lines Pedestrian Facilities (V1.3) Section 7.3	Lime marking to meet TfNSW Standards. Width 3.6m is adopted. The path adjoins 2-2.5m Shared Paths. Crossing width does not therefore need to be increased where warranted by high pedestrian volumes.

6.2 Proposed Pedestrian and Road Safety Improvements, Main Street and The Avenue, Alstonville

Ballina Shire Council

<p>Transport practice for numerical warrants for Pedestrian (Zebra) Crossings on arterial roads are: ii) Reduced Warrant for sites used predominantly by children and by aged or impaired pedestrians:</p> <p>If the crossing is used predominately by school children, is not suitable site for a Children's Crossing and in two counts of one hour duration immediately before and after school hours:</p> <p>(a) $P \geq 30$ AND (b) $V \geq 200$</p> <p>a pedestrian (Zebra) Crossing may be installed.</p> <p>If at least 50% of pedestrians using the crossing are aged or impaired and for each three one hour periods in a typical day</p> <p>(a) $P \geq 30$ AND (b) $V \geq 200$</p> <p>AND</p> <p>(c) $PV \geq 60,000$</p> <p>a pedestrian (Zebra) Crossing may be installed.</p> <p>iii) Special Warrant:</p> <p>In certain circumstances where:</p> <p>(a) $PV \geq 45,000$ but less than (60,000)</p> <p>AND</p> <p>(b) $P \geq 30$ AND (c) $V \geq 500$</p> <p>then consideration may be given to a potential pedestrian crossing site. In such circumstances, Transport must be satisfied with the additional reasons for why the location is in need of special consideration. Local traffic advice may be sought from council.</p>	<p>Non-arterial road but still considered for network consistency:</p> <p>13th September 2011:</p> <p>Morning 8am to 9am: 68 Children (136 count) 26 Adults 320 Vehicles $PV = (136+26) \times 320 = 51840$</p> <p>Afternoon 3:15pm to 4:15pm: 77 Children (154 count) 34 Adults 296 Vehicles $PV = (154+34) \times 296 = 55648$</p> <p>5th February 2024:</p> <p>Morning 32 Children (64 count) 24 Adults 7 Elderly (10 count) 264 Vehicles $PV = (64+24+10) \times 264 = 25872$</p> <p>Midday 2 Children (4 count) 13 Adults 2 Elderly (4 count) 172 Vehicles $PV = (4+13+4) \times 172 = 3612$</p> <p>Afternoon 75 Children (150 count) 27 Adults 1 Elderly (2 count) 282 Vehicles $PV = (150+27+2) \times 282 = 50478$</p> <p>Partially Met: LTC to accept as P and V warrants are met with the exception of $PV > 60\,000$, & $V > 500$ as $45\,000 > PV > 60\,000$</p> <p>These numbers do not factor in expected growth for DA approved new health facility (yet to be constructed) at 59 Main Street, which will increase elderly count at the proposed PX location.</p> <p>Refer below for current TfNSW Pedestrian Crossing Guidelines Warrants which are met in its entirety.</p>
<p>NSW practice does not permit Pedestrian (Zebra) Crossings on roads with two or more marked travel lanes in same direction. This also</p>	<p>Condition met</p>

6.2 Proposed Pedestrian and Road Safety Improvements, Main Street and The Avenue, Alstonville

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applies to roads with two unmarked travel lanes in the same direction (i.e. where vehicles can pass other vehicles travelling in the same direction)	
Pedestrian Crossings should be no less than 3.6m wide and must be indicated by pedestrian cross walk lines.	Condition Met
Refer supplementary document: TDT 2011/01a – Pedestrian Refuges and TDT2002/12c Stopping and Parking Restrictions at Intersections and Crossings	<p>7.5m no stopping for ≥ 2.5m kerb extension. No stopping enforced by the design of the kerb extension.</p> <p>No No Stopping signage is proposed.</p> <p>Existing No Stopping and No Parking signage are revised, detailed below.</p> <p>Minimum 15m long double barrier lines precede the RSP & Crossing on southern and western legs. RRPM at 5.0m spacing along the double barrier line are omitted due to narrowness as their lifespan and maintenance is compromised by their trafficability by all road users.</p>
Kerb Ramps	The RSP negates the need for kerb ramps
Zig Zag advance pavement marking	Sight Distance met, not required
<p>The Transport practice for locating Children's Crossings on local lightly trafficked roads are determined by:</p> <ul style="list-style-type: none"> o Traffic one hour duration immediately before and after school hours the traffic flow exceeds 50 vehicles per hour in each direction o One hour duration immediately before and after school hours 20 or more children cross the road within 20m of the proposed crossing location o The 85% percentile speed of traffic must not exceed 60km/h one hour before or after school hours 	<p>>50 vehicles per hour met</p> <p>20 or more children crossing met</p> <p>85% percentile speed met</p>

TfNSW Pedestrian Crossing Guidelines (2022) states the following in its preface, “Transport for NSW utilises a warrant system to determine if a pedestrian crossing is warranted at a specific location on roads operated by the NSW Government (Supplement to Australian Standard AS 1742.10-2009). This warrant system is not mandatory for use on non-arterial roads operated by local government.”

Both Main Street and The Avenue are non-arterial roads operated by council. TfNSW warrants are considered for due diligence and consistency in application of PX.

6.2 Proposed Pedestrian and Road Safety Improvements, Main Street and The Avenue, Alstonville

Ballina Shire Council

Table 4 addresses TfNSW Pedestrian Crossing Guidelines including council comments. Section 6.1 *Guidelines for all pedestrian crossings minimum criteria set out for pedestrian crossings in AS 1742.10* is omitted as it repeats warrants already covered in the above.

Table 4: TfNSW Pedestrian Crossing Guideline Requirements

TfNSW (2022) item	Comment
6.2.1 In each of two separate one-hour periods in a typical day, the pedestrian flow per hour (P) crossing the road is, or is expected to be, equal to or greater than 20. Children and elderly or mobility impaired pedestrians count as two pedestrians	Condition met
A road safety audit is recommended	Expected by Friday 23 rd of February 2024 via contractor GeoLINK PTY LTD
Subject to Local Traffic Committee processes	Purpose of this report.
Undertaken appropriate consultation and technical and design assessments, can provide supporting data, and have addressed foreseeable risks	Documented by this report. Current shared path widening directly west of this proposed PX is occurring. Letters to adjoining land owners of the shared path widening has been delivered. Informal advice was sought from the two school hours crossing guards at the two existing PX. Further Consultation via notification letters to occur prior to, at and during time of construction.
Pedestrian desire line	Existing desire line is benefited by proposed PX

Changes to Parking

Accessible Park

A previous investigation by Council's Manager – Engineering Works and Design Engineer to include an accessible park on level grade to the Alstonville Town Centre is implemented in this project. Other accessible parks in the vicinity require ambulant users to travel up or down a hill which can be undesirable. The accessible park does not include kerb ramps as it is flush with the footpath by means of localised kerb widening.

Regulated Parallel Parking

Formalised parking control varies in the vicinity with unregulated parallel parks, 5 minute parallel parks, 2 hour parallel parks, no parking zones which may be used for dropping off pedestrians, and 10m no stopping zones about the intersections. This is consolidated in the proposal to only include 5 minute parallel parks, 2 hour parallel parks and minimum regulatory no stopping zones.

Figure 5 identifies the proposed changes to parking.

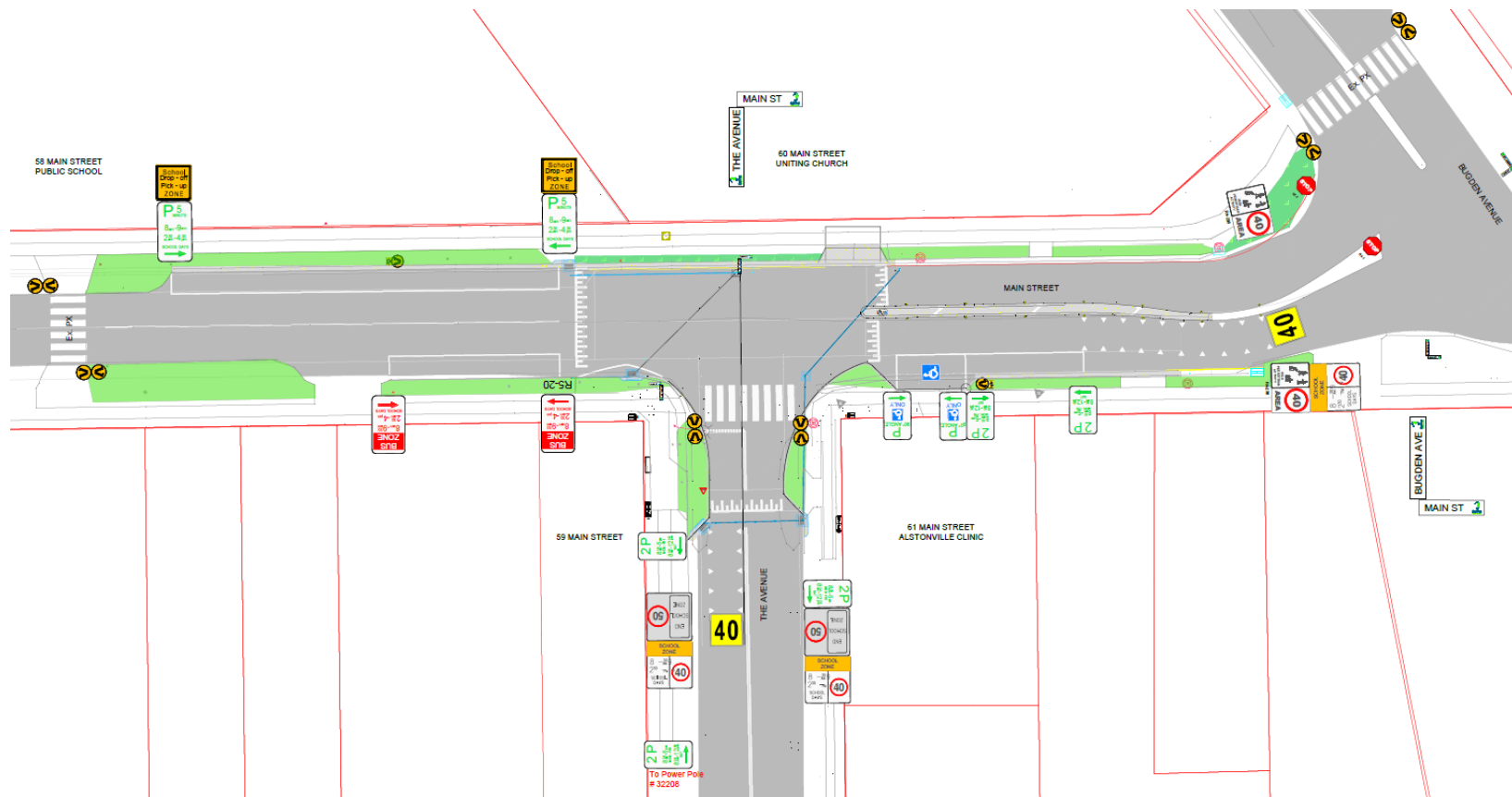


Figure 5: Regulatory Signage and line marking included in the proposal

Outcome:

The TfNSW Supplement to Australian Standard AS 1742.10-2009 warrants for the implementation of a pedestrian crossing of over 20 pedestrians for two one hour periods per day with vehicle volume exceeding 200 are met. Counts from 2011 show that the criteria has been met for some time. Known development and alignment to strategic documentation and the implementation of the pedestrian upgrades is expected to increase active transport in increasing these numbers.

The second TfNSW Supplement criteria of the multiplication of Pedestrian times Vehicle is almost met at 50 000 not quite to the required 60 000, which the LTC is recommended to accept.

The current TfNSW Pedestrian Crossing Guideline warrants are readily met.

A formalised crossing consistent to the scope of the funding source is recommended.

Next Step:

Following approval by the LTC for the implementation of the proposed PX the following should occur:

In consultation with the Alstonville Public and St Joseph's School Principal's apply to TfNSW for the appointment of a third School Crossing Supervisor to manage this new PX. Crossing Supervisors already manage the adjoining two crossings.

Further information: [School crossing supervisors | Transport for NSW](#)

Application: [School crossing supervisor form | Transport for NSW](#)

Extracted from the further information:

The School Crossing Supervisor Program will assess the nominated site against the following set criteria:

The site must have an existing children's crossing, pedestrian crossing (zebra) or combined crossing (children's and zebra) on a public road. [Condition met](#)

Infant and/or primary school children must use the crossing. [Condition met](#)

The site must be within a 40km/h school zone. [Condition met](#)

In the morning or afternoon, the crossing must register counts of either:

50 or more unaccompanied infant and/or primary school children [Condition met](#)

300 or more passenger car units (heavy vehicles over 3 tonnes unladen count as two passenger car units).

[Assumed, crossings either side of this one already have Crossing Supervisors](#)

The site must be considered a safe working environment for a school crossing supervisor. [Low speed intersection.](#)

Appendix I

Scope Extract from GFR-1056 Funding Letter

3. Scope of Works and Approvals

Council is responsible for delivery of the scope of works as listed in their approved funding application.

Council is responsible for submitting the relevant project design and construction documentation for any required reviews and obtaining all project design and construction approvals required for completion of the project. This includes, but is not limited to all:

- Transport design and construction reviews and approvals for all works on State assets (Traffic Control Plans, works on State roads, etc);
- Any approvals required under the Roads Act (Traffic Committee, etc);
- Any project planning approvals required under the planning approval pathways in NSW (REF, Heritage, etc);
- Council design and construction reviews and approvals on local roads;
- Service relocation design reviews and approvals from Service Authorities (water, electrical, comms, etc); and
- Cost estimates, if required.

Where required, Council is to undertake all stakeholder engagement and consultation on the project.

Council is responsible for undertaking and documenting project risk assessments as required. Risk mitigation strategies must be developed and implemented if required.

If required, Council is to undertake a Road Safety Audit consistent with the *Guidelines for Road Safety Audit Practices*, and address all corrective actions. Any audits must be undertaken by an accredited and independent audit team.

Transport will work in collaboration with Council to review proposed designs of Council projects. For projects funded under the Get NSW Active Program, additional support services from Transport may be available to assist Councils with advice and input on designs and approvals. Transport encourages Council project managers to contact the Active Transport Representative to access this support.

Council is responsible for ensuring that the project does not conflict with any other current/upcoming projects or construction/works in the area.

A1.1

Appendix II

2011 Pedestrian Count

The Avenue
Alstonville
Pedestrian &
Vehicle Count
in The Avenue
at Daley Street
Int

Time	High School	Primary school	Primary (Accompanied)	Adults	Cars
0800-0815	4	3	3	7	50
0815-0830	15	9	1	3	65
0830-0845	12	5	1	4	85
0845-0900	11	2	2	5	120
0900-0915	2	4	5	7	70
0915-0930	1	0	2	4	40
0930-0945	0	0	0	2	41
0945-1000	0	0	0	1	40
1430-1445	0	0	0	0	47
1445-1500	0	0	0	1	53
1500-1515	0	0	1	4	62
1515-1530	0	12	15	13	78
1530-1545	35	5	0	1	98
1545-1600	6	0	2	1	51
1600-1615	2	0	0	2	69
1615-1630	0	0	0	0	61
-					
-					

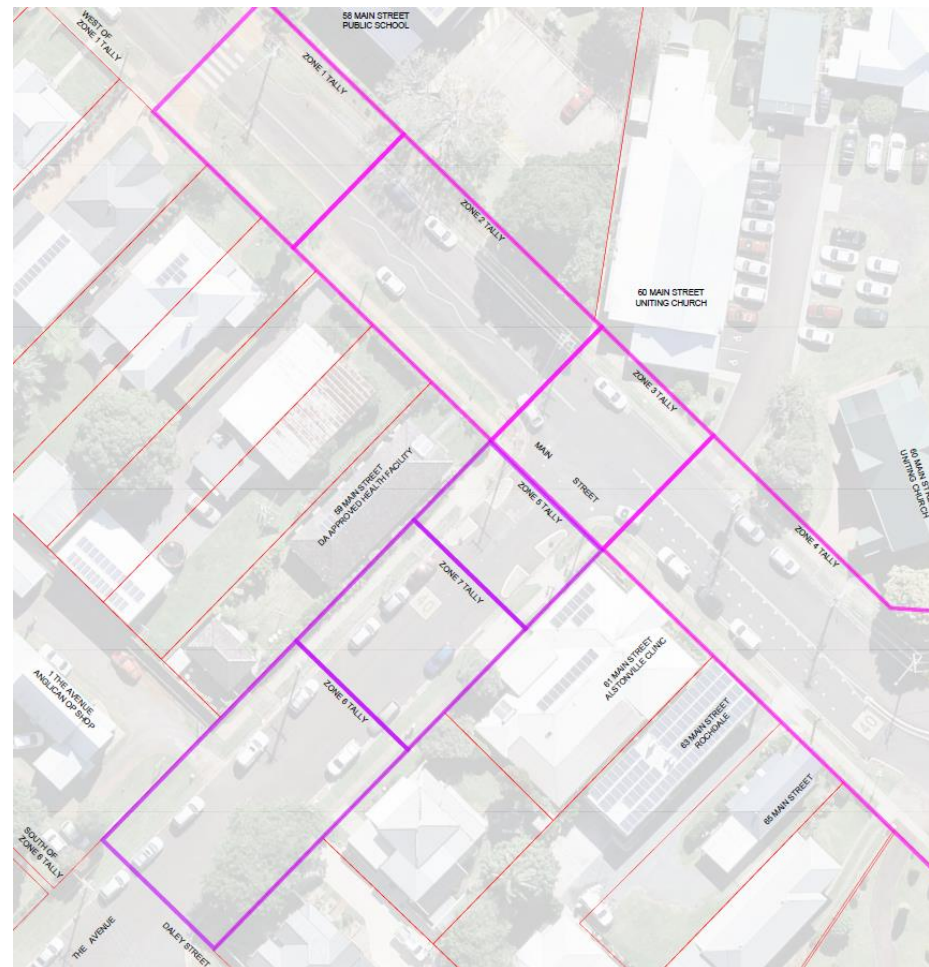
WEATHER -

A2.1

Appendix III

2024 Pedestrian Count Zones:

0. West of the Main Street School PX
1. Main St. School PX
2. Between Zone 1 and The Avenue
3. At the Main St. The Ave. Intersection
4. Between Zone 3 and Bugden Ave.
5. Existing Informal Crossing at Kerb Extensions
6. Southern Half The Ave. North of Daley St.
7. Northern Half The Ave. North of Daley St.



A3.1

6.2 Proposed Pedestrian and Road Safety Improvements, Main Street and The Avenue, Alstonville

Compiled Count Data

Date	Time	Zone 0 (West of Zone 1)				Zone 1				Zone 2				Zone 3				Zone 4			
		Normal	Child	Elderly	Sum	Normal	Child	Elderly	Sum	Normal	Child	Elderly	Sum	Normal	Child	Elderly	Sum	Normal	Child	Elderly	Sum
2/02/2024	8:00-8:15	1			1		1		1		1		1	1			1	5	5	1	11
	8:15-8:30	1			1	3	12		15				0				0	3	1		4
	8:30-8:45	2			2	6	25		31		4		4				0	2			2
	8:45-9:00		1		1	12	22		34	2			2	1			1	1			1
5/02/2024	8:00-8:15				0			2	2				0				0	2	7		9
	8:15-8:30		6		6		16	2	18				0	1		1	2		2	1	3
	8:30-8:45	1	2		3	4	39	1	44				0				0	1			1
	8:45-9:00				0	23	16	2	41				0				0			1	1
5/02/2024	12:00-12:15				0	1			1				0				0	1		1	2
	12:15-12:30		1		1	1	2		3				0				0				0
	12:30-12:45				0				0				0				0	2		1	3
	12:45-13:00		1		1				0				0				0	2			2
	15:00-15:15	2			2	16	36		52	1			1				0				0
	15:15-15:30				0	6	16		22				0				0				0
5/02/2024	15:30-15:45				0	3	34		37				0				0	2			2
	15:45-16:00				0				0				0				0		1		1
	16:00-16:15				0				0				0				0				0
	16:15-16:30				0				0				0				0				0

A3.2

6.2 Proposed Pedestrian and Road Safety Improvements, Main Street and The Avenue, Alstonville

Engineering Report The Avenue & Main Street, Alstonville
Get NSW Active 2023/24

		Zone 5 (Proposed Crossing)				Zone 6				Zone 7				Zone 8 South of Daley St			
Date	Time	Normal	Child	Elderly	Sum	Normal	Child	Elderly	Sum	Normal	Child	Elderly	Sum	Normal	Child	Elderly	Sum
2/02/2024	8:00-8:15	2	3		5	1			1				0				0
	8:15-8:30	3	4		7	4		2	6	1	2		3				0
	8:30-8:45	4	9		13	1			1	2	2		4	1	3		4
	8:45-9:00	3	6		9	2	1		3				0				0
5/02/2024	8:00-8:15	4	5	2	11	2			2				0				0
	8:15-8:30		9		9		2	1	3				0				0
	8:30-8:45	5	9		14	2	3		5	1	1	1	3				0
	8:45-9:00	10	7	1	18	2			2	3	1	2	6				0
5/02/2024	12:00-12:15	1			1	2			2	1		1	2				0
	12:15-12:30	3	2		5	2			2	3			3				0
	12:30-12:45	2			2	3			3				0				0
	12:45-13:00	1			1	5			5	2		1	3				0
	15:00-15:15	19	16		35	3			3				0				0
	15:15-15:30	3	31		34	4	3		7	1			1	2	4		6
5/02/2024	15:30-15:45	2	20	1	23				0				0	1	3		4
	15:45-16:00	2	8		10				0				0				0
	16:00-16:15				0				0				0				0
	16:15-16:30				0				0				0				0

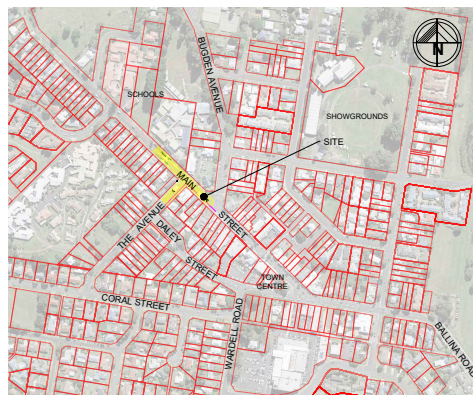
SR40.001/4 MAIN STREET & THE AVENUE INTERSECTION UPGRADE - GET NSW ACTIVE 2023/2024

PM24/10320

ISSUE A JANUARY 2024

TENDER DRAWINGS

DRAWING	SHEET	ISSUE	DATE	TITLE
SR40.001/4...DWG	COVER	A	JAN 24	COVER SHEET (THIS SHEET)
	1 OF 10	B	JAN 24	NOTES
	2	A	JAN 24	GENERAL PLAN
	3	A	JAN 24	MC01 MAIN STREET DETAIL PLAN
	4	A	JAN 24	MC02 MK01 & MK02 DETAIL PLAN
	5	A	JAN 24	MK03 MAIN STREET MEDIAN DETAIL PLAN
	6	A	JAN 24	DRAINAGE LINE DL1 & DL2 DETAIL PLAN
	7	A	JAN 24	DL3 DETAIL PLAN
	8	A	JAN 24	MC01 MAIN St. CROSS SECTIONS CH: 420.00 TO 450.00
	9	A	JAN 24	MC01 MAIN St. CROSS SECTIONS CH: 451.13 TO 490.00
	10	A	JAN 24	MK04 MAIN St. CROSS SECTIONS CH: 495.00 TO 522.422
	11	B	JAN 24	SIGNS, LINE MARKING & FURNITURE PLAN
12	A	JAN 24	SEDIMENT, EROSION CONTROL & SAFEGUARDS PLAN	



LOCALITY PLAN

WARNING

BEWARE OF UNDERGROUND SERVICES

THE LOCATIONS OF UNDERGROUND SERVICES ARE APPROXIMATE ONLY AND BASED ON DBYD THEIR EXACT POSITION MUST BE PROVEN ON SITE. NO GUARANTEE IS GIVEN THAT ALL EXISTING SERVICES ARE SHOWN.

CONSTRUCTION SITE KNOWN TO CONTAIN:
OVERHEAD & UNDERGROUND POWERLINES
TELSTRA & NBN ROAD CROSSINGS AND CABLES
BALLINA SHIRE COUNCIL STORMWATER
BSC WATER, GRAVITY SEWER
STATE SURVEY & CADASTRE MARKS

REFERENCE DRAWING	ISSUE	DATE
R-03	B	18/02/2009
R-05	D	06/03/2018
R-06	D	24/10/2012
R-08	A	03/10/06
SW-02	B	08/03/2018
TfNSW R0200-47 Break in median	-	January 2017
SP071 BSC - Street Blade Specifications	V2.0	01/06/2020

ballina shire council

GENERAL

- G1. ALL DRAWINGS UNDER THIS PACKAGE SHALL BE READ IN CONJUNCTION WITH THE NORTHERN RIVERS LOCAL GOVERNMENT STANDARD DRAWINGS AND CONSTRUCTION SPECIFICATIONS.
- G2. ALL DUTIES AND WORKMANSHIP SHALL BE IN ACCORDANCE AND COMPLIANCE IN PREFERENCE TO:
 - A. THESE DRAWINGS
 - B. NORTHERN RIVERS LOCAL GOVERNMENT DEVELOPMENT SPECIFICATIONS AND STANDARD DRAWINGS.
 - C. RELEVANT AND CURRENT STANDARDS OF AUSTRALIA.
- G3. ANY DISCREPANCY SHALL BE REFERRED TO THE SUPERINTENDENT BEFORE PROCEEDING WITH THE WORKS.
- G4. UNLESS NOTED OTHERWISE (UNO):
 - A. ALL DIMENSIONS ARE IN METRES.
 - B. ALL LEVELS ARE IN METERS TO AUSTRALIAN HEIGHT DATUM (AHD).
- G5. ALL DIMENSIONS SHOWN ON THE DRAWINGS ARE NOMINAL ONLY. THE CONTRACTOR SHALL VERIFY THE DIMENSIONS ON SITE PRIOR TO FABRICATION OR CONSTRUCTION. DRAWINGS SHALL NOT BE SCALED FOR DIMENSIONS.
- G6. NO SUBSTITUTE MATERIALS SHALL BE USED WITHOUT THE WRITTEN APPROVAL OF COUNCIL.
- G7. THE CONTRACTOR SHALL PROVIDE ADEQUATE TEMPORARY PROTECTION AND SITE FENCING TO PREVENT ENTRY OF UNDESIRABLE PERSONS AND ANIMALS DURING CONSTRUCTION.
- G8. PRIOR TO COMMENCEMENT OF SITE CLEARANCE AND EXCAVATION, THE CONTRACTOR SHALL MAKE ALL ENQUIRIES AND INVESTIGATIONS TO PROTECT FROM DAMAGE ALL ASSETS & SERVICES.
- G9. EXISTING SERVICES:
 - A. THE CONTRACTOR SHALL BE RESPONSIBLE FOR PROTECTING THE EXISTING SERVICES. VERIFICATION OF THE LOCATION OF THESE SERVICES MUST BE OBTAINED PRIOR TO ANY EXCAVATION COMMENCING.
 - B. INFORMATION OF THE EXISTING SERVICES ARE SHOWN IN THE DRAWINGS IN GOOD FAITH. NO GUARANTEE IS GIVEN OR IMPLIED THAT SUCH INFORMATION IS ACCURATE OR COMPLETE. IT IS THE RESPONSIBILITY OF THE CONTRACTOR TO DETERMINE THE POSITION OF THE EXISTING SERVICES BEFORE COMMENCING CONSTRUCTION. THE CONTRACTOR SHALL BE RESPONSIBLE FOR ANY DAMAGE TO THE EXISTING SERVICES DURING THE COURSE OF THE CONTRACT.
 - C. THE CONTRACTOR TO OBTAIN PRIOR APPROVAL FROM COUNCIL FOR ANY DEVIATIONS REQUIRED TO AVOID EXISTING SERVICES.

ROAD PAVEMENT AND EARTHWORKS

- R01 ALL EXCAVATION, BACKFILLING AND COMPACTION SHALL BE CARRIED OUT IN ACCORDANCE WITH AS 3798-2007 AND NLRG C213 EARTHWORKS SPECIFICATION.
- R02 ALL FLEXIBLE PAVEMENTS SHALL BE CONSTRUCTED IN ACCORDANCE WITH NLRG C242 FLEXIBLE PAVEMENT SPECIFICATION.
- R03 ALL BITUMEN SPRAY SEAL WEARING SURFACE SHALL BE IN ACCORDANCE WITH NLRG C244 PRAYED BITUMINOUS SURFACING SPECIFICATION.
- R04 ALL ASPHALTIC CONCRETE WEARING SURFACE SHALL BE IN ACCORDANCE WITH NLRG C245 ASPHALTIC CONCRETE SPECIFICATION.
- R05 CLIPPING AND DISTURBED VEGETATION SHALL BE KEPT TO A MINIMUM AND SHALL BE LIMITED TO AREAS CONTAINING THE EXCAVATIONS FOR THE CONSTRUCTION OF THE REQUIRED WORKS.
- R06 MINIMUM COMPACTION SHALL BE:
SELECT FILL - 95% STANDARD MAXIMUM DRY DENSITY AT +/- 2% MODIFIED OPTIMUM MOISTURE CONTENT
SUBGRADE - 95% STANDARD MAXIMUM DRY DENSITY AT +/- 2% MODIFIED OPTIMUM MOISTURE CONTENT
SUBBASE - 100% STANDARD MAXIMUM DRY DENSITY AT +/- 2% MODIFIED OPTIMUM MOISTURE CONTENT
BASE - 100% STANDARD MAXIMUM DRY DENSITY AT +/- 2% MODIFIED OPTIMUM MOISTURE CONTENT
- R07 SUBGRADE PREPARATION SHALL BE CARRIED OUT AS FOLLOWS:
1. EXCAVATE AND REMOVE ANY TOPSOIL, ORGANIC MATTER, ROOT AFFECTED MATERIAL AND FILL TO SUBGRADE LEVEL WITH THE SPOILING OF DELETERIOUS MATERIALS
2. RIP THE EXPOSED SUBGRADE TO A MINIMUM OF 300mm BELOW DESIGN LEVEL AND COMPACT TO THE LIM SPECIFIED IN RD6 ABOVE.
3. PROOF ROLL THE PREPARED SUBGRADE IN ACCORDANCE WITH NLRG SPECIFICATIONS. ANY AREAS IDENTIFIED AS BEING SOFT OR WEAK DURING THE PROOF ROLL THAT DO NOT RESPOND TO FURTHER COMPACTION SHOULD BE EXCAVATED AND REPLACE WITH SELECT FILL TO A MINIMUM DEPTH OF 300mm.
SELECT FILL SHALL BE PLACED IN LAYERS NOT EXCEEDING 150mm LOOSE THICKNESS AND BE COMPACTED IN ACCORDANCE WITH RD6 ABOVE.
4. ANY ABRUPT CHANGES BETWEEN SUBGRADE CONDITIONS (EG. EXISTING PAVEMENTS AND RESIDUAL SOL) SHALL BE SMOOTHED AND REPAIRED.
5. GRADING AND GRADING SHALL BE UNDERTAKEN TO TRANSITION BETWEEN SUBGRADE CONDITIONS.
6. ALL FILL MATERIAL PLACED SHALL BE COMPACTED AND TRIMMED TO MATCH WITH THE FINAL EARTHWORKS LEVELS AND PROFILES SHOWN ON THE DRAWINGS.
- R010 EARTHWORKS COMPACTION AND PAVEMENTS SHALL BE INSPECTED AFTER THE INSTALLED IN ACCORDANCE WITH THE APPLIED INSPECTION AND TESTING PLAN COMPLYING WITH THE GRC CQC QUALITY CONTROL REQUIREMENTS SPECIFICATION
- R011 ALL TOPSOIL STRIPPED FROM WORK AREAS SHALL BE STOCKPILED IN AN APPROVED LOCATION FOR LATER RE-SPREADING. RE-SPREAD SHALL BE TO A MINIMUM DEPTH OF 100mm to the NLRG 150mm
- R012 PAVEMENT MARKINGS SHALL BE INSTALLED AND APPLIED IN ACCORDANCE WITH NLRG C281 PAVEMENT MARKINGS SPECIFICATION

CONCRETE AND REINFORCING

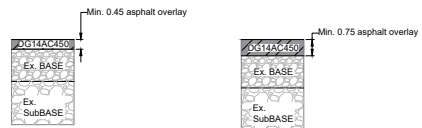
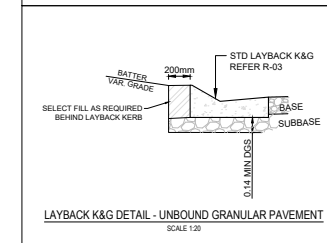
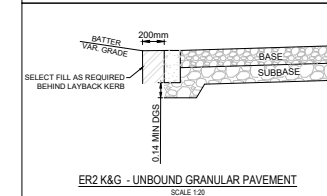
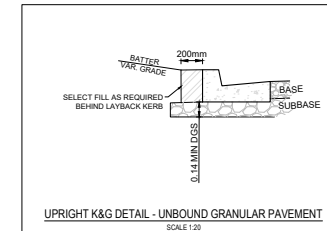
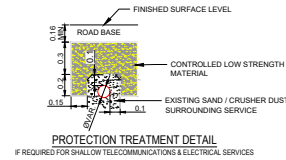
- C1. ALL CONCRETE WORKMANSHIP AND MATERIALS SHALL BE IN ACCORDANCE WITH AS 3600 AND THE NRLC C248 PLAN OR REINFORCED CONCRETE BASE SPECIFICATION.
- C2. CONCRETE GRADE FOR SPECIFIC STRUCTURES UNO SHALL BE AS FOLLOWS
C2.1 ALL STRUCTURES UNLESS SPECIFIED OTHERWISE N25/20
C2.2 SCOUR PROTECTION N20/20
- C3. THE MAXIMUM SIZE OF AGGREGATE IN THE CONCRETE SHALL BE 20mm
- C4. CONCRETE MIX DESIGN INCLUDING PROPORTIONS OF ADDITIVES AND CEMENTITIOUS REPLACEMENT MATERIALS, SHALL BE SUBMITTED TO THE SUPERINTENDENT FOR APPROVAL PRIOR TO PLACEMENT.
C4.1 ALL LAYERS MUST NOT BE USED IN EXCESS OF 150mm
- C5. A MIX DESIGN USING A HIGHER SLUMP AND SMALLER AGGREGATE SIZE SHALL BE CONSIDERED BY THE SUPERINTENDENT WHERE CONCRETING IS DONE BY PUMPING.
- C6. MINIMUM CLEAR CONCRETE COVER TO ALL REINFORCEMENT SHALL 50mm UNLESS NOTED OTHERWISE
- C7. SIZES OF CONCRETE ELEMENTS SHOWN ON THE DRAWINGS DO NOT INCLUDE THICKNESS OF APPLIED FINISHES
- C8. THE FINISHED CONCRETE SHALL BE A DENSE HOMOGENEOUS MASS, COMPLETELY FILLING THE FORMWORK, THOROUGHLY EMBEDDING THE REINFORCEMENT AND FREE OF ANY STONE POCKETS OR VOIDS. CONCRETE SHALL BE VIBRATED BY MECHANICAL VIBRATORS DURING PLACEMENT.
- C9. ALL FORMED EXPOSED EDGES HAVING A CONTAINED ANGLE OF LESS THAN 120° AND RE-ENTRANCES SHALL BE BELLFINED OR FILED TO REMOVE SHARP CORNERS
- C10. NO PENETRATIONS, CHASES OR TEMPORARY FIXTURES OTHER THAN THOSE SHOWN ON THE DRAWINGS ARE PERMITTED IN THE CONCRETE MEMBERS WITHOUT PRIOR APPROVAL OF THE SUPERINTENDENT.
- C11. CONSTRUCTION JOINTS SHALL BE PROPERLY FORMED AND USED ONLY WHERE SHOWN ON THE DRAWINGS. CONSTRUCTIONALLY CAST JOINTS SHALL BE SUITED TO SUPPORT SURFACES AGAINST WHICH NEW CONCRETE IS TO BE PLACED SHALL BE CLEAN, FREE OF ALL LAITANCE, AND BE DAMPENED DOWN PRIOR TO COMMENCEMENT OF POURING NEW CONCRETE.
- C12. ALL CONCRETE INTERFACES ARE TO BE ROUGHENED TO ENSURE SATISFACTORY BOND BETWEEN INSITU CONCRETE AND PRECAST CONCRETE OR BETWEEN DIFFERENT FOURS OF INSITU CONCRETE UNO. ALL CONCRETE SURFACES TO BE FREE OF LAITANCE.

EROSION AND SEDIMENT CONTROL


- E51. ALL SOIL EROSION AND SEDIMENT CONTROL MEASURES SHALL BE INSTALLED PRIOR TO THE COMMENCEMENT OF ANY WORKS BEING CARRIED OUT. ALL SOIL EROSION MEASURES SHALL BE MAINTAINED AND KEPT IN PLACE FOR THE FULL DURATION OF THE WORKS AND SHALL ONLY BE REMOVED AT FINAL STABILISATION OF THE WORKS, WHERE IT IS NECESSARY TO UNDERTAKE STRIPPING IN ORDER TO CONSTRUCT A SEDIMENT CONTROL DEVICE ON SUFFICIENT GROUND SHALL BE STRIPPED TO ALLOW CONSTRUCTION.
- E52. ALL SOIL EROSION AND SEDIMENT CONTROL MEASURES AS INDICATED ON THESE DRAWINGS ARE DIAGRAMMATIC ONLY AND THE ACTUAL REQUIREMENTS SHALL BE CONFIRMED ON SITE PRIOR TO COMMENCEMENT.
- E53. ALL DRAINAGE WORKS SHALL BE CONSTRUCTED AND STABILISED AS QUICKLY AS POSSIBLE TO MINIMISE RISK OF EROSION.
- E54. DUST AND SITE DISTURBANCE MUST BE KEPT TO A MINIMUM ALWAYS. DURING WINDY WEATHER, LARGE UNPROTECTED AREAS MUST BE KEPT MOST (NOT WET) BY SPRINKLING WITH WATER TO REDUCE WIND VELOCITY.
- E55. PLACE ALL STOCKPILES AWAY FROM AREAS OF LIKELY CONCENTRATED OR HIGH VELOCITY FLOWS, ESPECIALLY EARTH BANKS AND ROADS; IF NECESSARY, EARTH BANKS OR DRAINS WILL BE CONSTRUCTED TO DIVERT LOCALISED RUN-ON. STOCKPILE LOCATIONS SUBJECT TO COUNCIL APPROVAL.
- E56. AVOID REVERSING THE SOIL PROFILE MATERIALS DURING FILL OPERATIONS - REPLACE DISTURBED SOILS IN THEIR ORIGINAL ORDER.
- E57. LEAVE TOPSOIL IN A SCARIFIED OR ROUGH CONDITION ONCE REPLACED TO HELP MOISTURE INFILTRATION



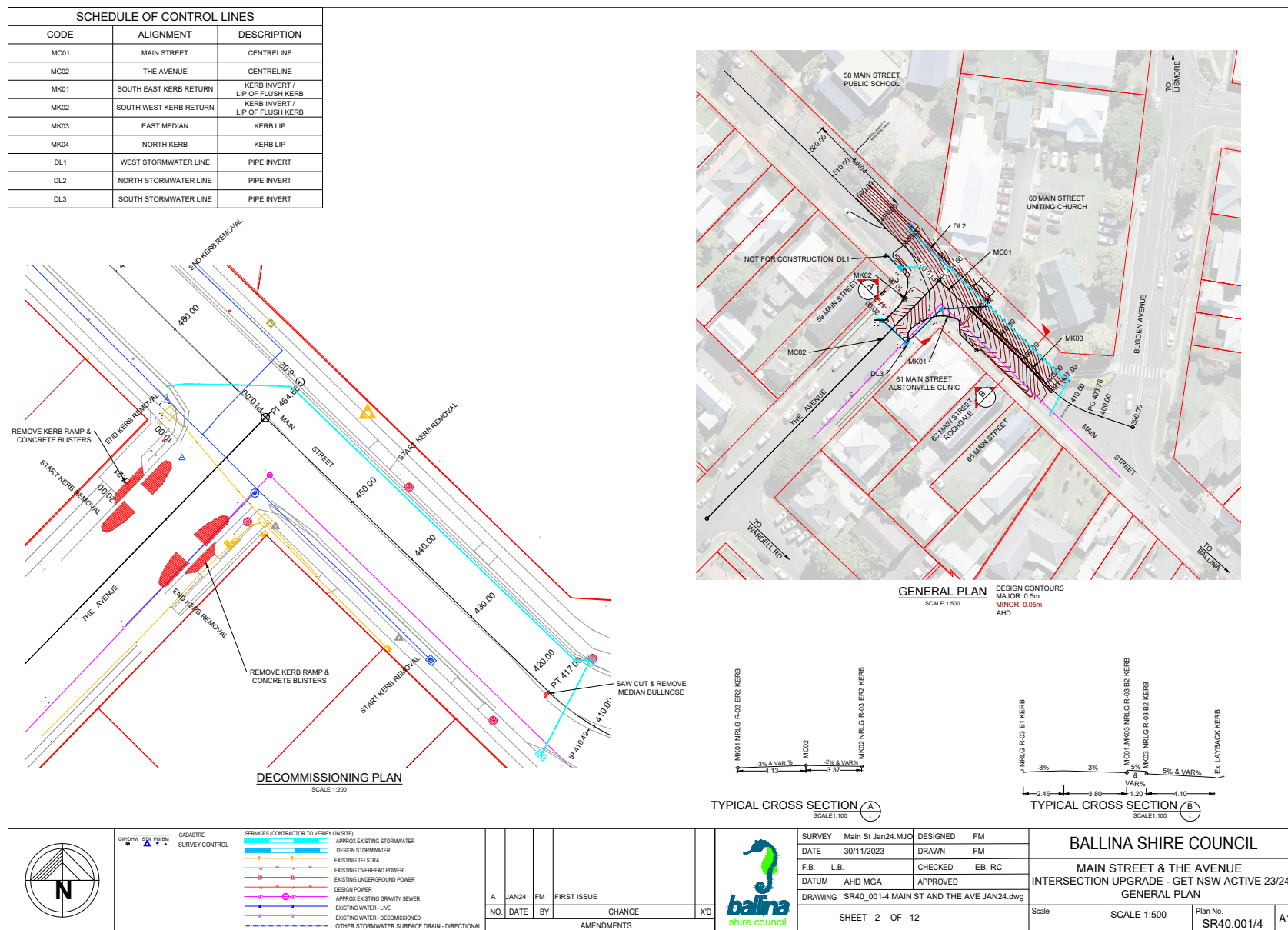
PAVEMENT DESIGN PLAN
SCALE 1:1000

PAVEMENT DESIGN - ASPHALT CORRECTION
NOT TO SCALE

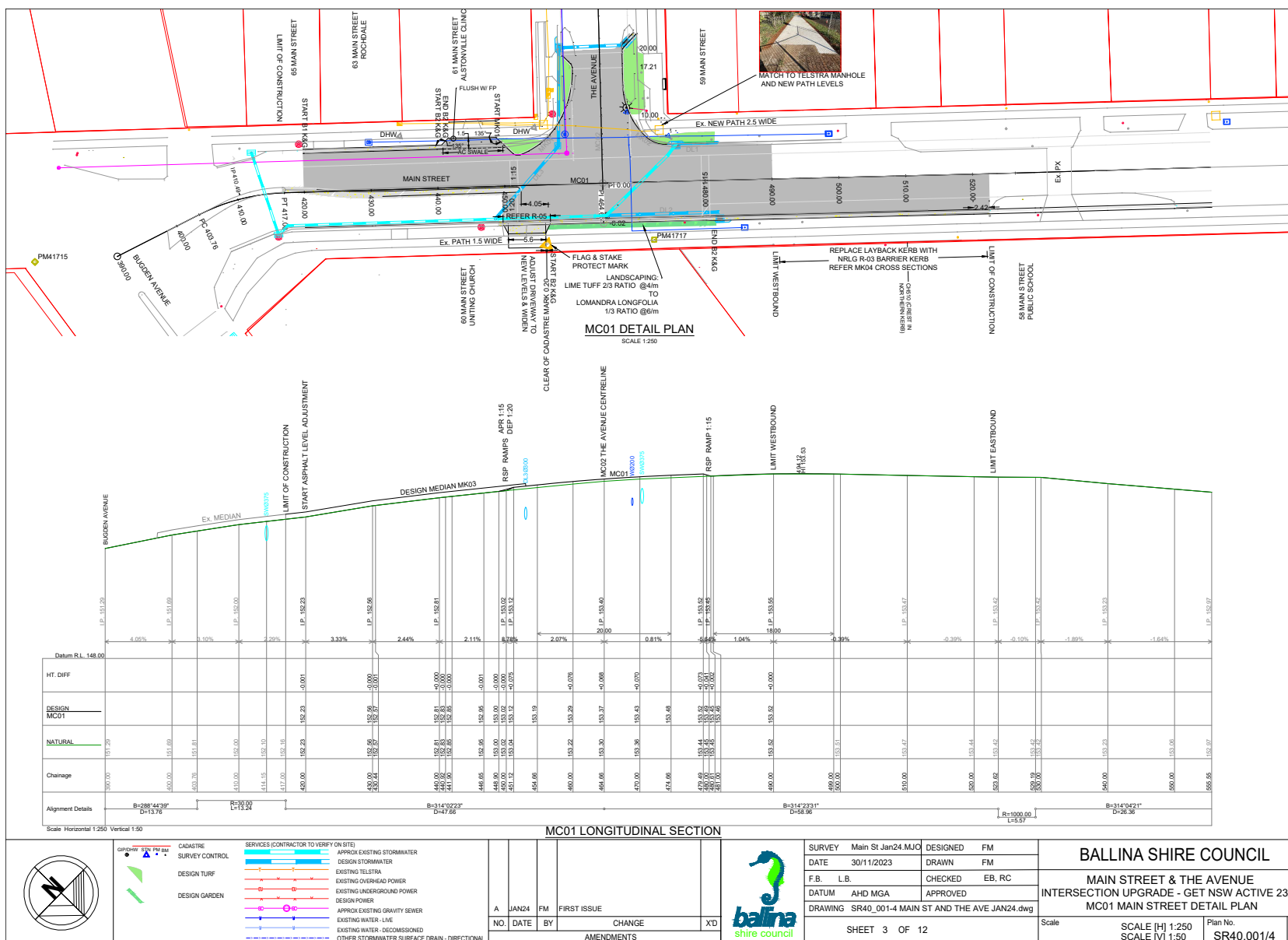
PAVEMENT DESIGN - RSF

						SURVEY Main St Jan24.MJC DATE 30/11/2023 F.B. L.B. DATUM AHD MGA DRAWING SR40_001-4 MAIN ST AND THE AVE JAN24.dwg		DESIGNED FM DRAWN FM CHECKED EB, RC APPROVED SHEET 1 OF 11		BALLINA SHIRE COUNCIL MAIN STREET & THE AVENUE INTERSECTION UPGRADE - GET NSW ACTIVE 23/24 NOTES	
B A JAN24 JAN24 FM FM NO. DATE BY AMENDMENTS				PAVEMENT DESIGN PLAN ADDED FIRST ISSUE CHANGE XD				Scale		Plan No. SR40.001/4	

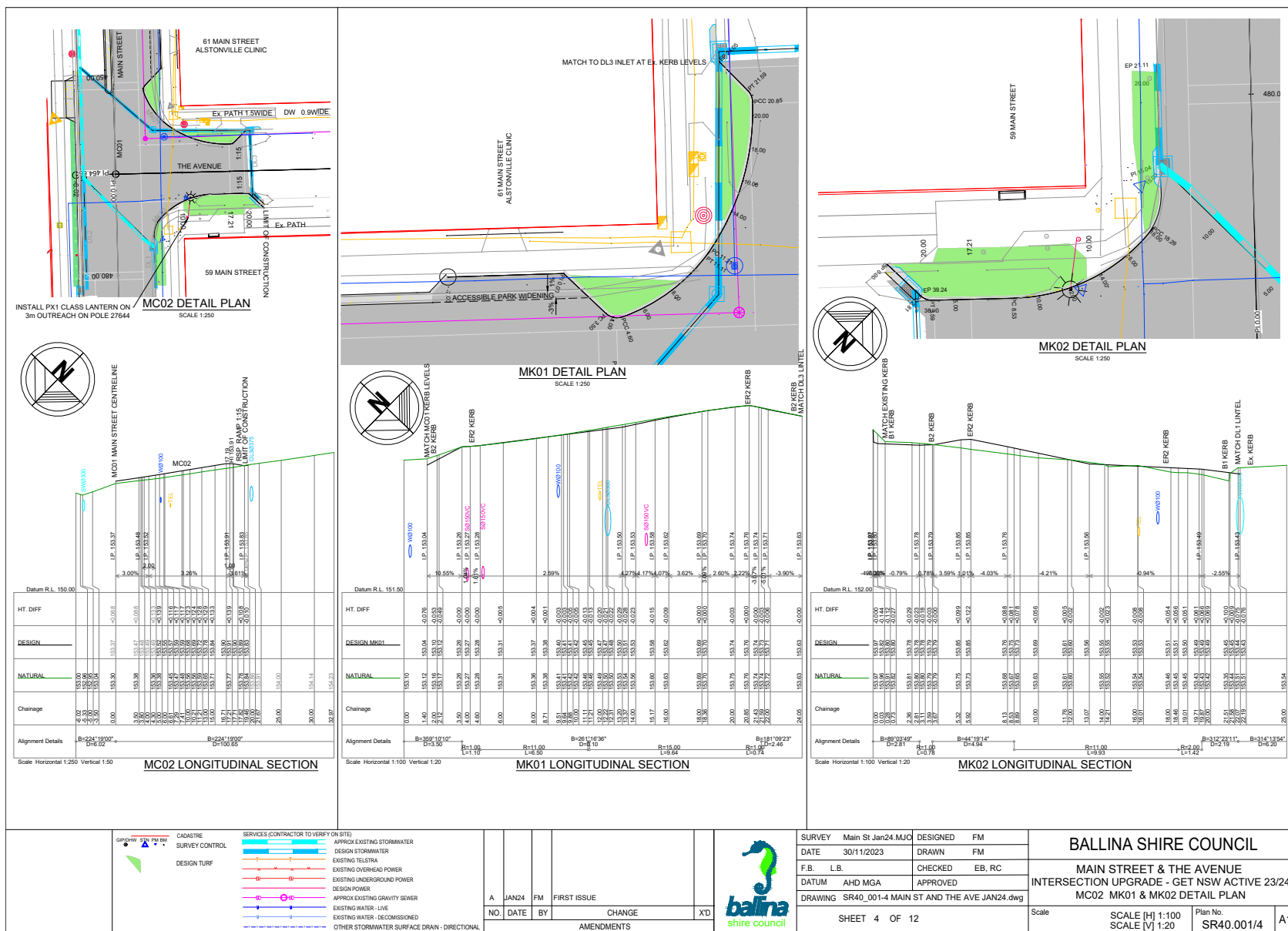
6.2 Proposed Pedestrian and Road Safety Improvements, Main Street and The Avenue, Alstonville



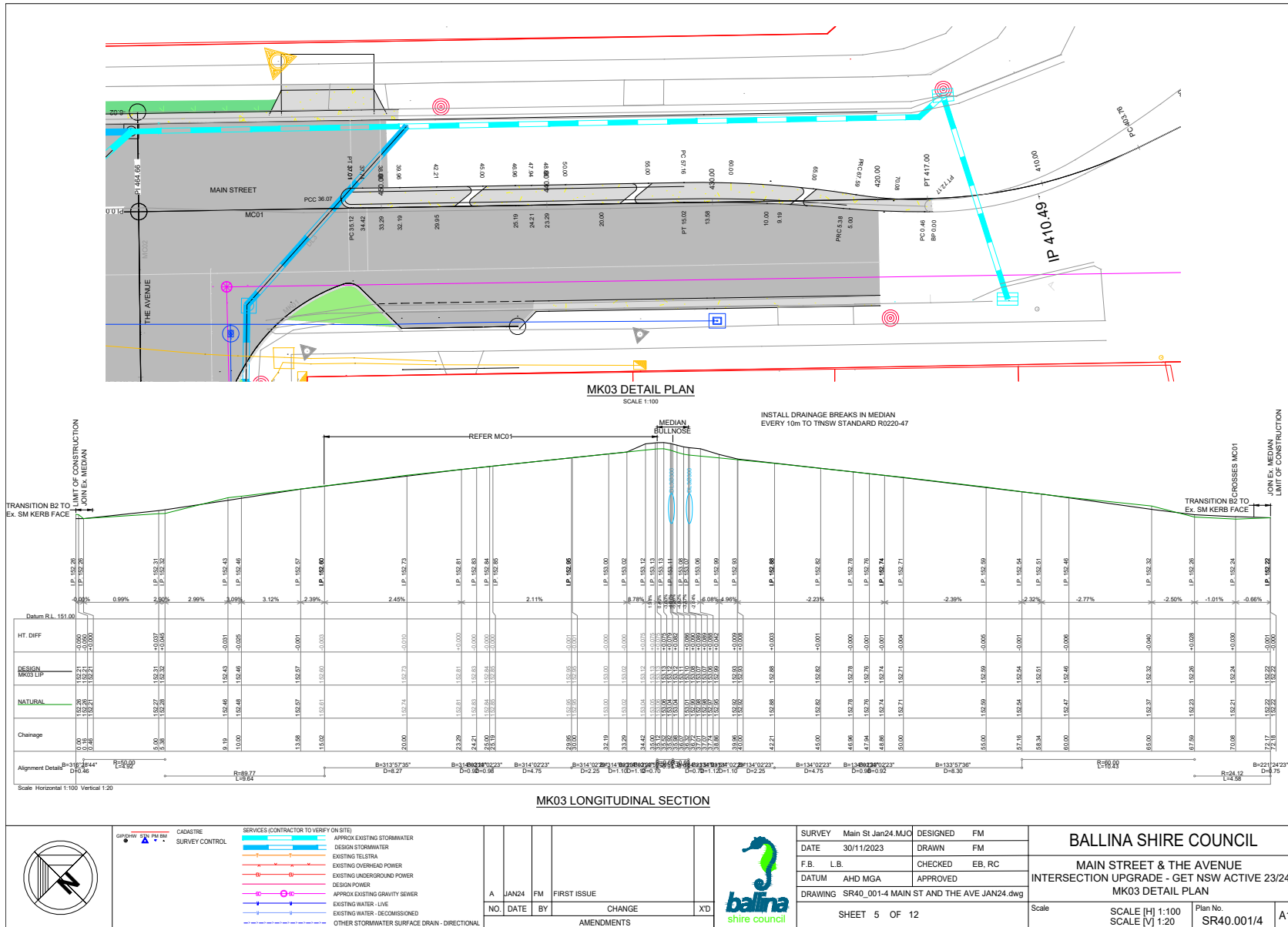
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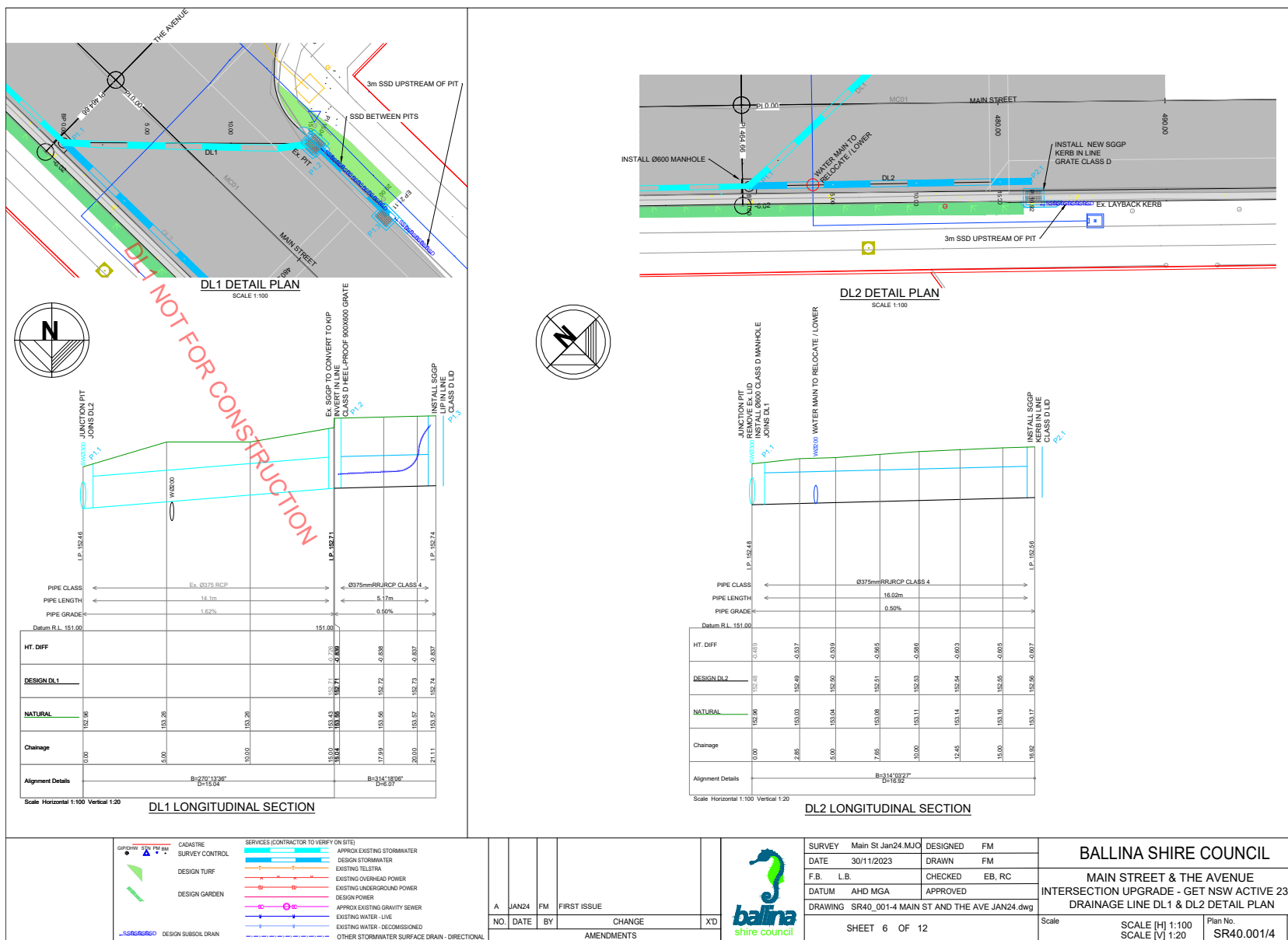
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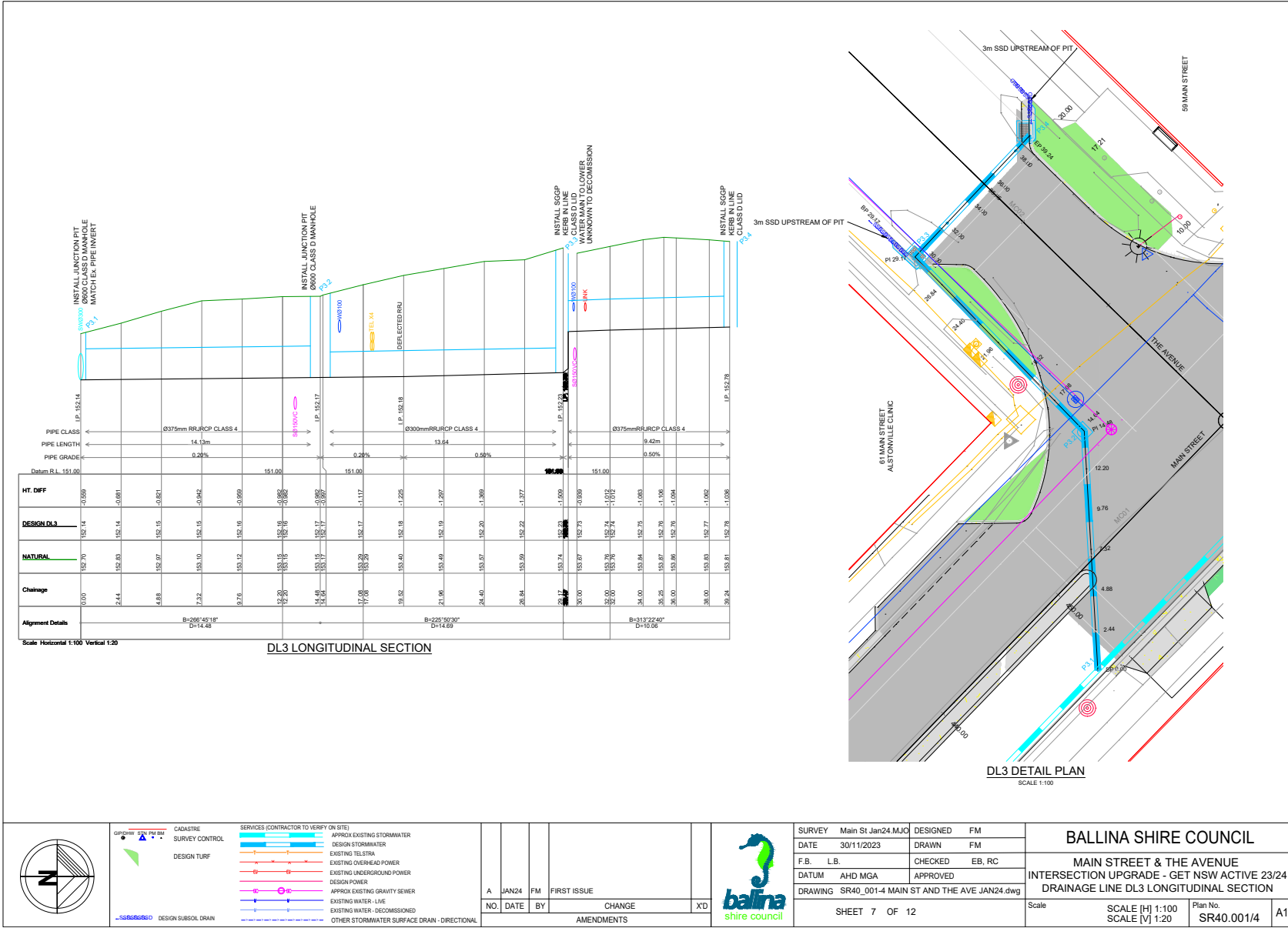
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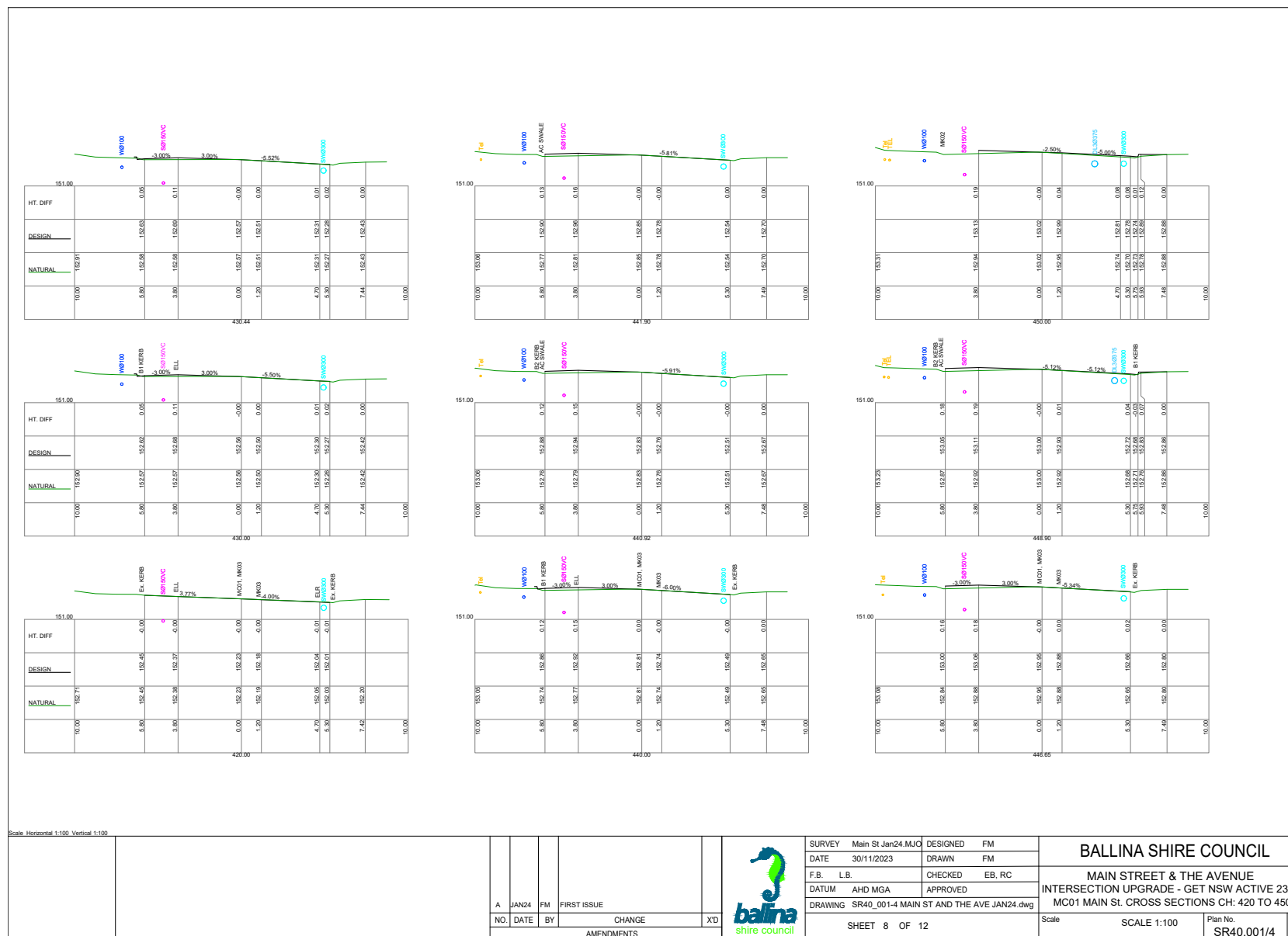
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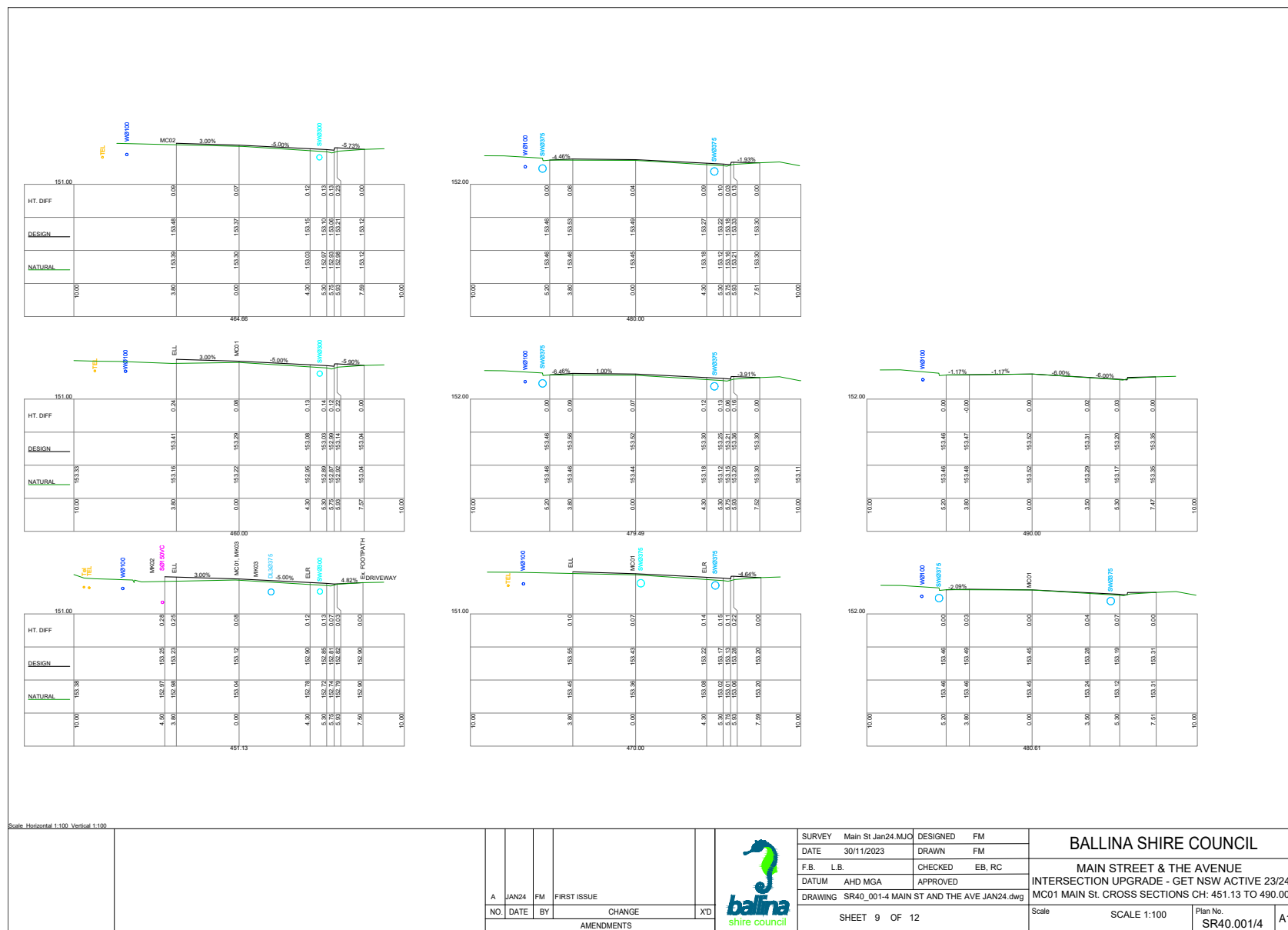
6.2 Proposed Pedestrian and Road Safety Improvements, Main Street and The Avenue, Alstonville



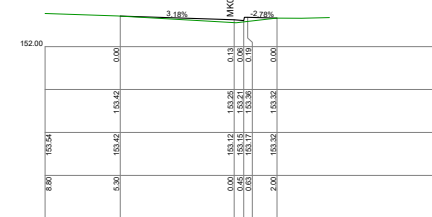
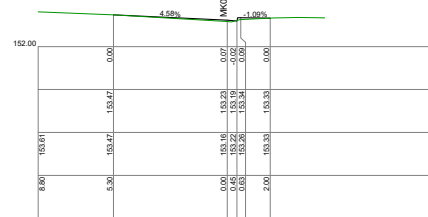
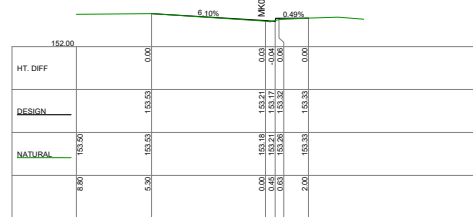
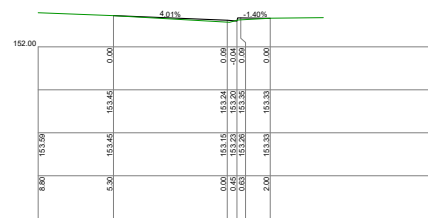
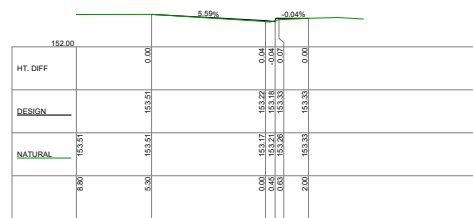
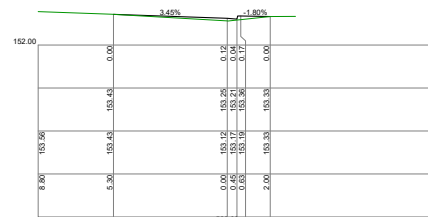
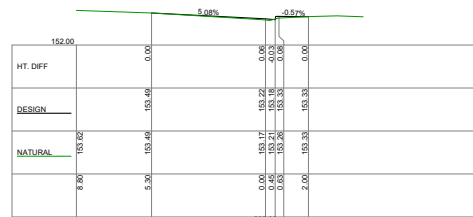
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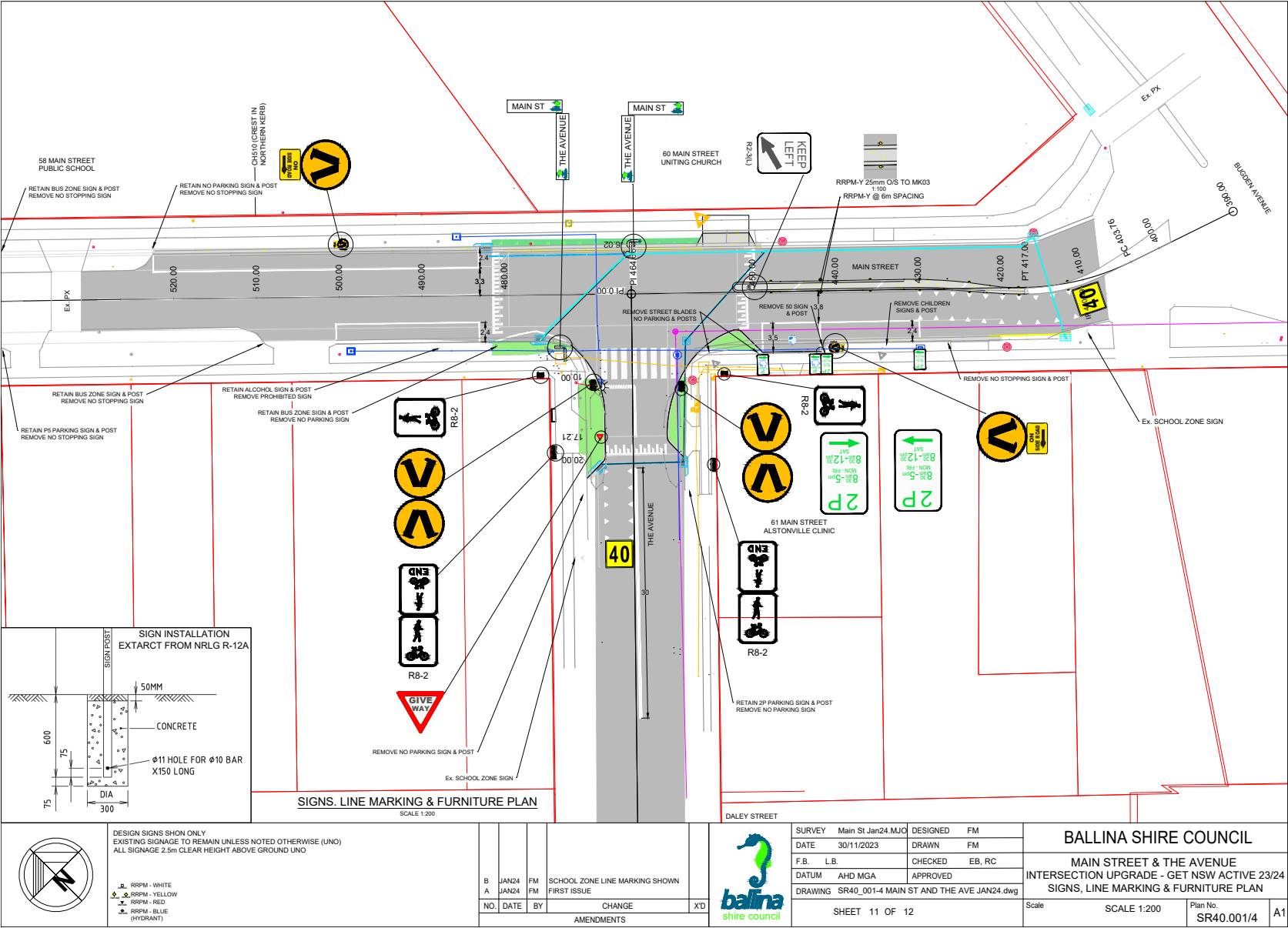


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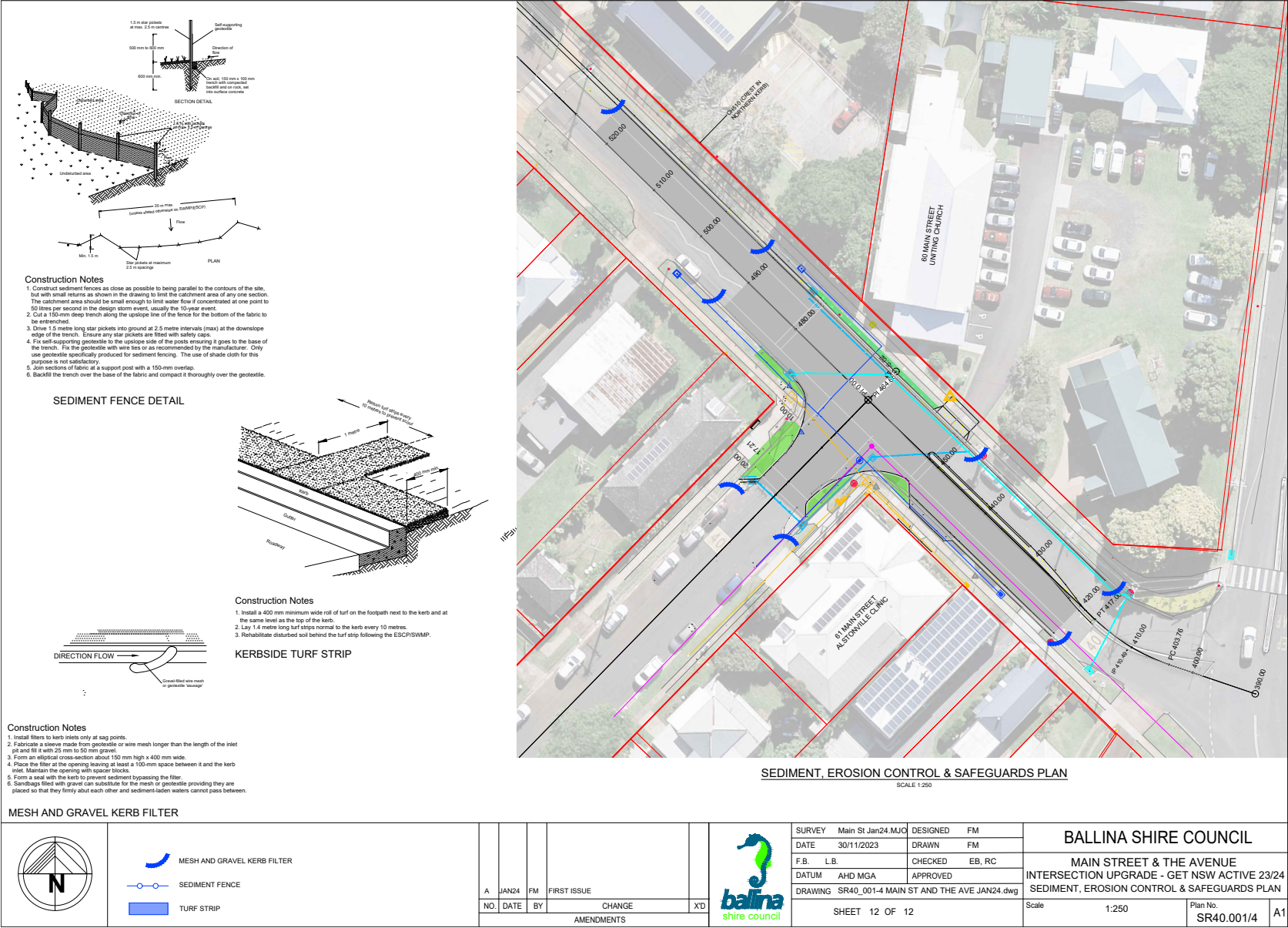
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											DATE 30/11/2023		DRAWN FM										
											F.B. L.B.		CHECKED EB, RC		MAIN STREET & THE AVENUE INTERSECTION UPGRADE - GET NSW ACTIVE 23/24 MK04 MAIN SL CROSS SECTIONS CH: 495.00 TO 522.422								
											DATUM AHD MGA		APPROVED										
											DRAWING SR40_001-4 MAIN ST AND THE AVE JAN24.dwg												
											SHEET 10 OF 12												
																Scale		SCALE 1:100		Plan No.			
																		SR40.001/4		A1			

7/23/2024 11:14:28 AM, ISO file saved A1 (Set 00 x Set 00/500)

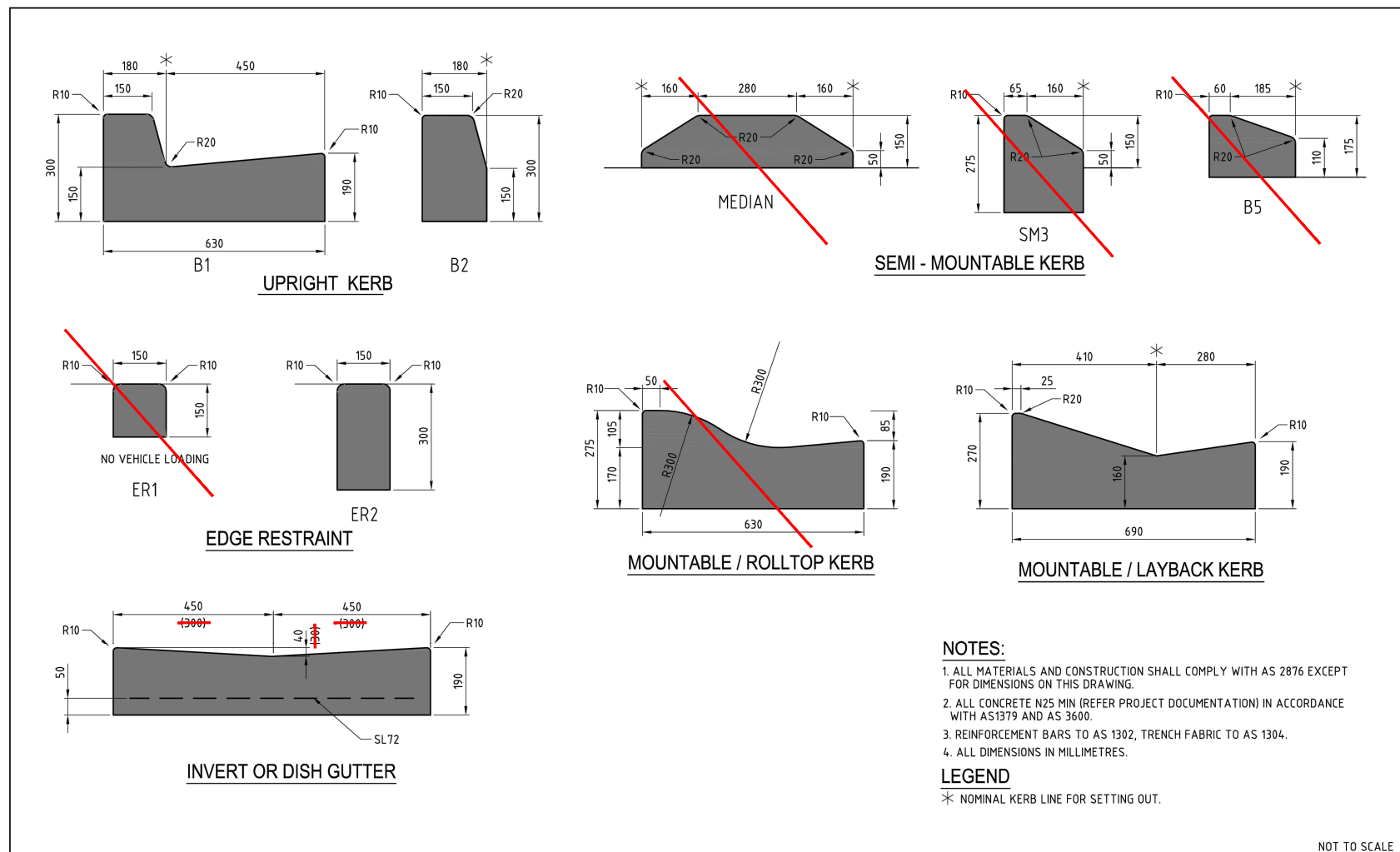
6.2 Proposed Pedestrian and Road Safety Improvements, Main Street and The Avenue, Alstonville



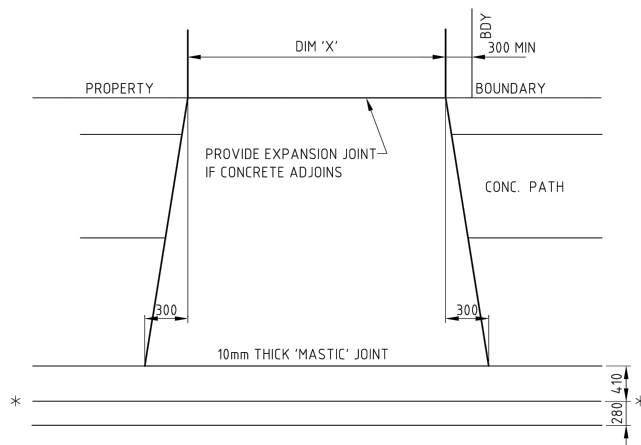
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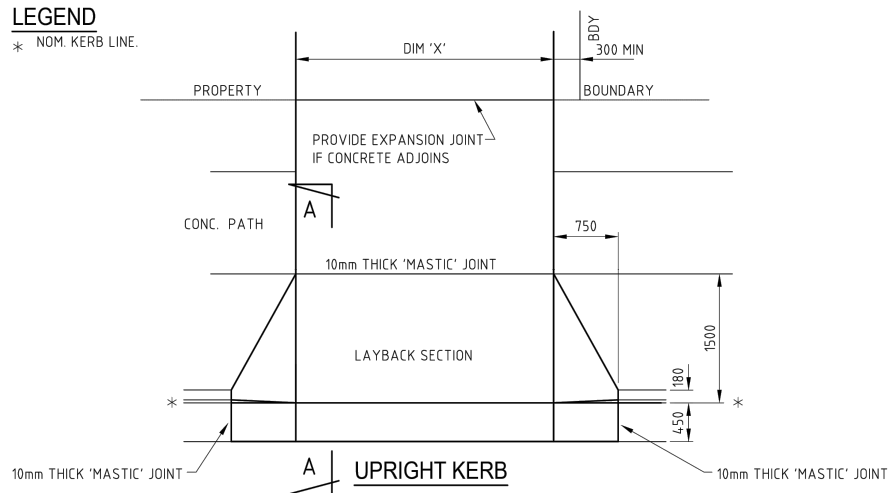
				STANDARD DRAWINGS		KERBS AND GUTTERS, EDGE RESTRAINTS, MEDIAN & INVERT PROFILES AND DIMENSIONS	ROAD/STREET STANDARD DRAWING R-03
				NORTHERN RIVERS LOCAL GOVERNMENT			
B	MOUNTABLE ROLLTOP KERB ADDED		18/2/09				
A	ORIGINAL ISSUE		3/10/06				
REVISIONS		APP'D	DATE				
A	B						



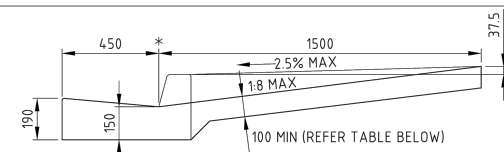
MOUNTABLE KERB

LEGEND

* NOM. KERB LINE.



UPRIGHT KERB



LAYBACK VEHICLE CROSSING SECTION A-A

NOTES:

1. THESE DETAILS ARE FOR TYPICAL URBAN SITUATIONS. ANY VARIATIONS FROM THESE STANDARDS SHALL REQUIRE SUBMISSION OF DETAILED ALTERNATIVE DESIGN & GRADING TO COUNCIL FOR APPROVAL PRIOR TO COMMENCEMENT OF WORKS
2. FOOTPATH SECTION TO VARY WHERE NECESSARY TO MATCH CONCRETE FOOTPATHS AND VERGE PROFILES. FOOTPATH EARTHWORKS ADJOINING CONCRETE MUST BE WELL COMPACTED.
3. CONCRETE SURFACE TOLERANCE TO BE $\pm 5\text{mm}$ OVER 3 METRE SECTIONS.
4. CONCRETE N25 IN ACCORDANCE WITH AS 1379 AND AS 3600.
5. REINFORCEMENT FABRIC TO AS 1304, 50 TOP AND EDGE COVER, LAP FABRIC 250.
6. EXPANSION JOINTS TO BE 10 THICK, FULL DEPTH CLOSED CELL CROSS LINKED POLYETHYLENE FOAM (85 - 150 KG/M³) OR 10MM THICK COMPRESSED GRANULATED CORKBOARD, INSTALLATION TO MANUFACTURERS' INSTRUCTIONS.
7. A STREET OPENING PERMIT OR DRIVEWAY APPLICATION MUST BE OBTAINED FROM COUNCIL. THE APPLICANT SHALL OBTAIN APPROVAL OF LOCATION & LEVELS PRIOR TO EXCAVATION
8. REFER TO STANDARD DRAWING NO R14 FOR RURAL OR W.S.U.D. APPLICATIONS
9. GRADE NOT TO EXCEED 1 IN 14 WHERE DISABILITY ACCESS IS REQUIRED.
10. ALL DIMENSIONS IN MILLIMETRES.
11. EXISTING FOOTPATH PROFILE TO BE MAINTAINED WHERE POSSIBLE. THE COST OF ANY ADJUSTMENTS OR VARIATIONS SHALL BE BORNE IN FULL BY THE APPLICANT
12. COMPACTION FOR SUBGRADE 95% STANDARD TO AS 1289.5.1.1.
13. CONCRETE DRIVEWAY UNLESS OTHERWISE APPROVED
14. PAVER AND/OR PROPRIETARY CONCRETE SURFACE FINISHES TO DRIVEWAY SHALL ONLY BE USED WHEN APPROVED BY LOCAL AUTHORITY.

TYPE OF DRIVEWAY	WIDTH AT BOUNDARY 'X' mm	THICKNESS 'T' mm @ REINFORCEMENT
SINGLE DWELLING OR DUPLEX UNIT	3000 min. 4000 max	100mm with SL62
UNITS LOCAL STREETS	REFER TO A.S.2890	150mm with SL72
* UNITS COLLECTOR ROADS	REFER TO A.S.2890	150mm with SL72
* COMMERCIAL/ * INDUSTRIAL	REFER TO A.S.2890	200mm with SL82

NOT TO SCALE

REVISIONS	APP'D	DATE
D	NOTES GENERALLY UPGRADED	MPK 6/3/2018
C	NOTES GENERALLY UPGRADED	18/6/2012
B	NOTES GENERALLY UPGRADED	7/7/10
A	ORIGINAL ISSUE	3/10/06

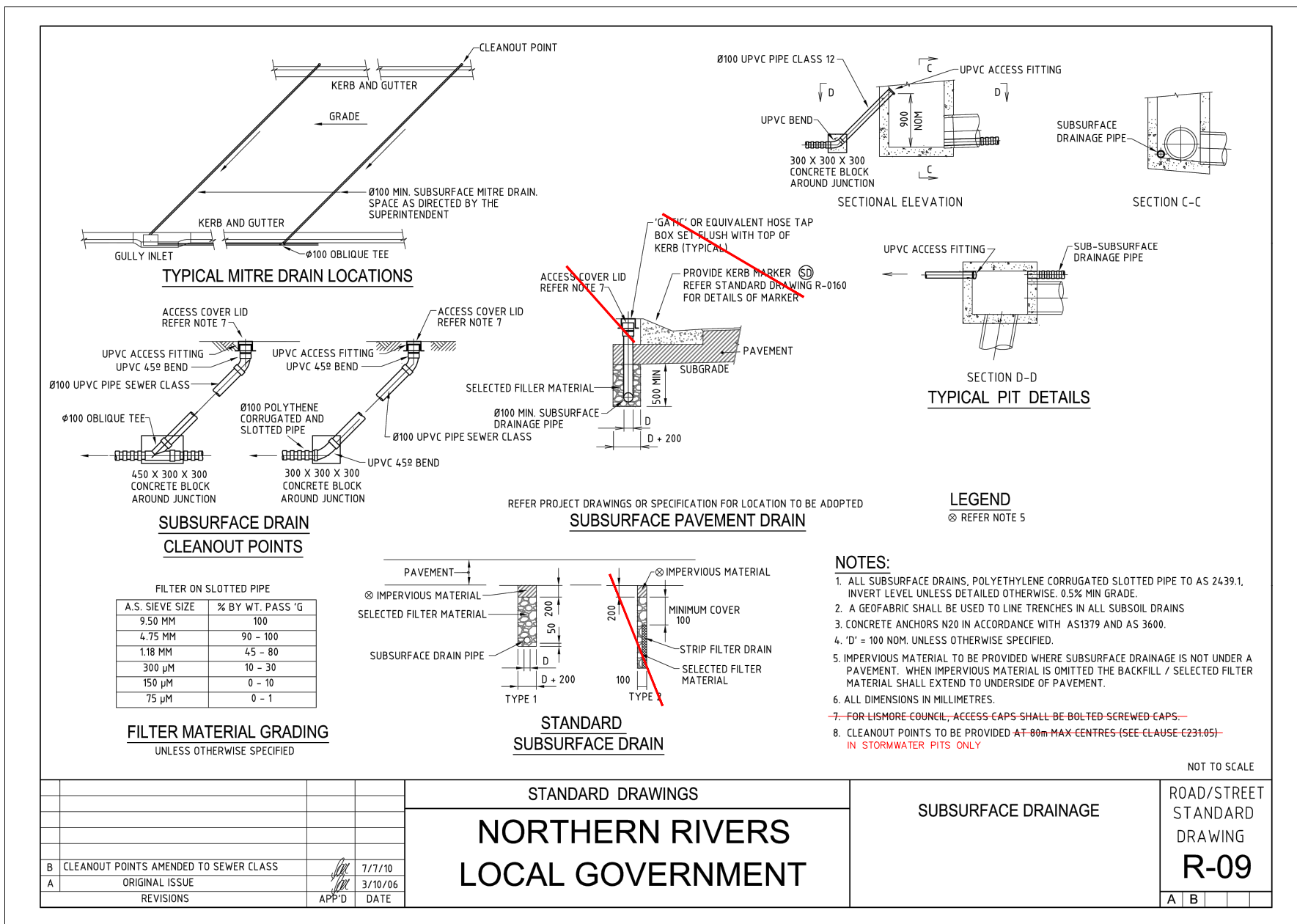
STANDARD DRAWINGS

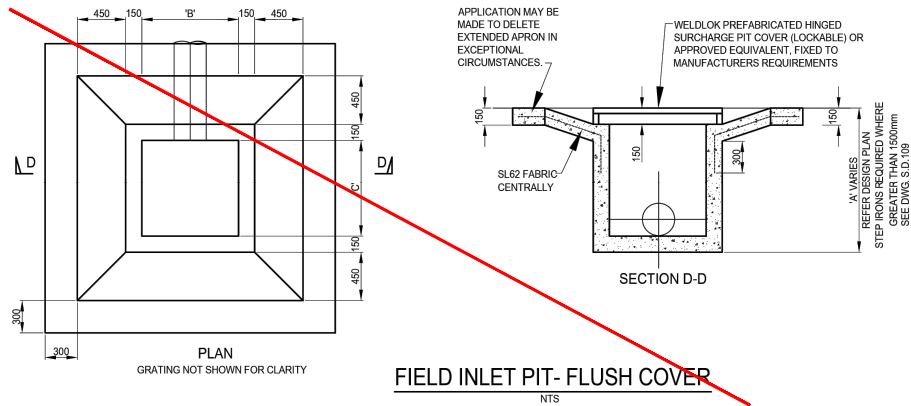
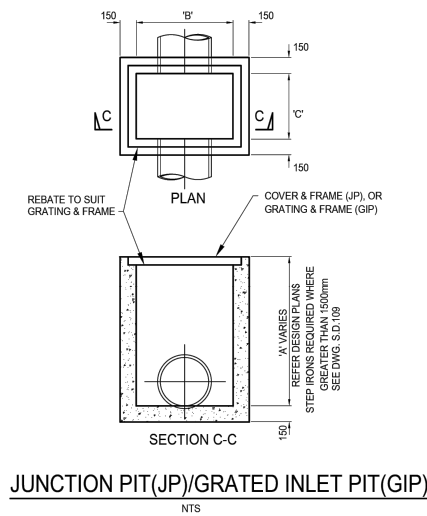
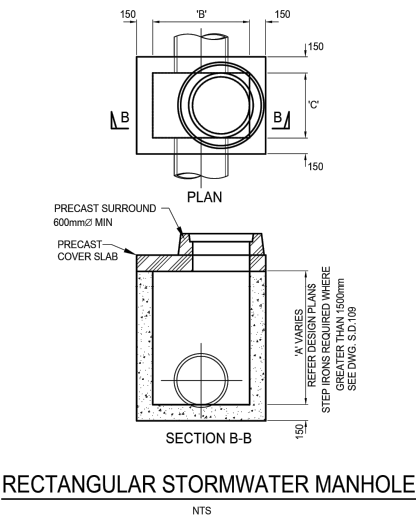
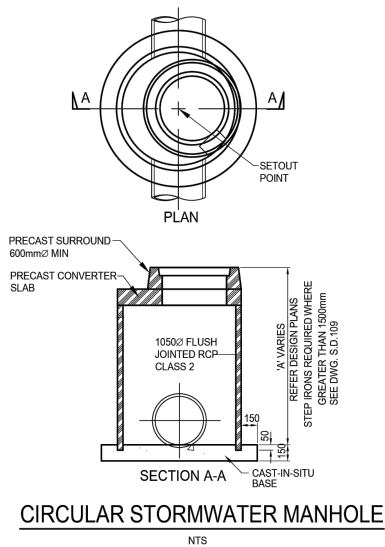
NORTHERN RIVERS LOCAL GOVERNMENT

URBAN RESIDENTIAL DRIVEWAY LAYOUTS
& LAYBACK VEHICULAR CROSSING
FOR KERBED ROADS

ROAD/STREET
STANDARD
DRAWING
R-05

A B C D





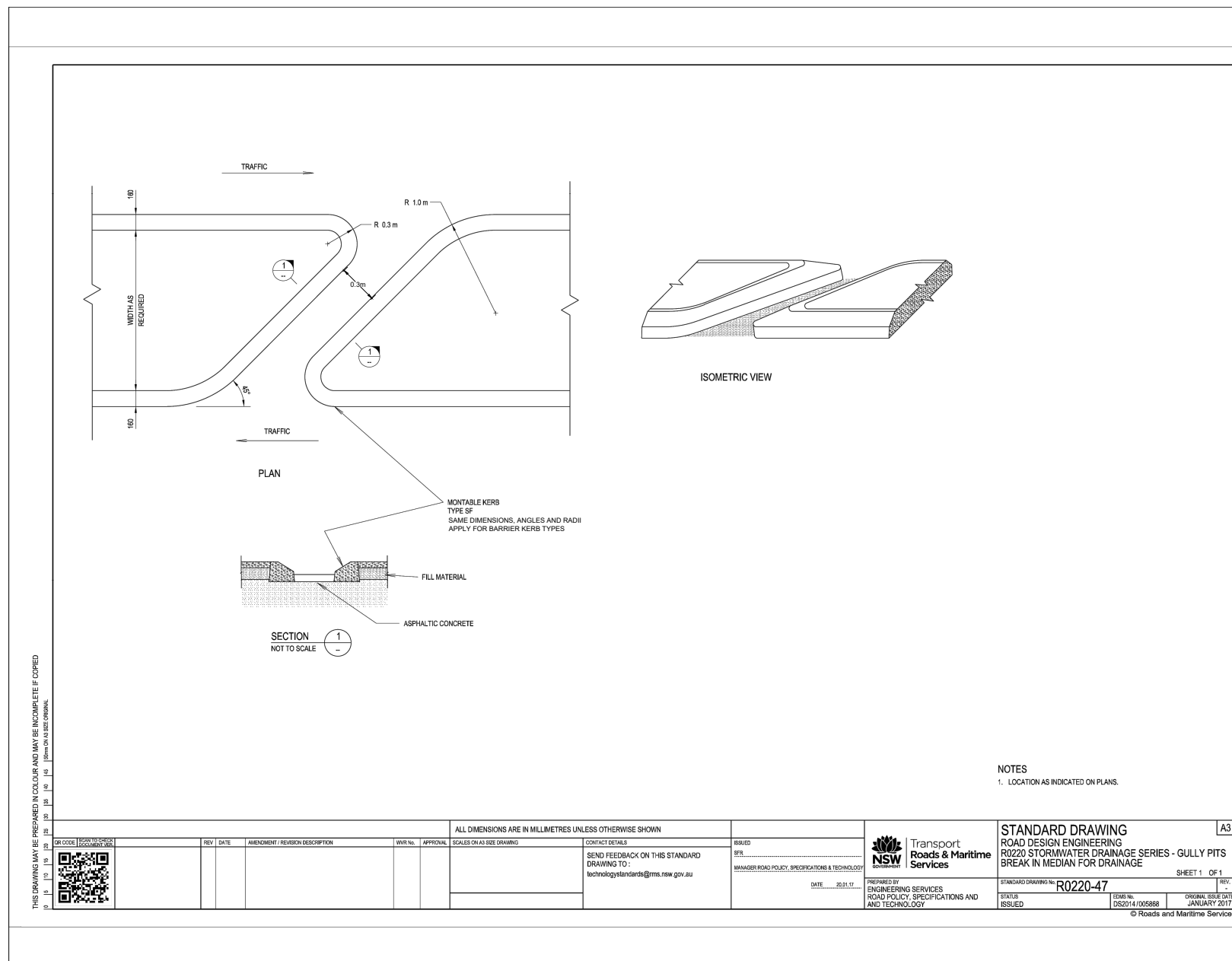
B	AMENDMENTS TO PIT DIMENSIONS	MPK	08/03/18
A	ORIGINAL ISSUE	<i>[Signature]</i>	7/7/10
	REVISIONS	APP'D	DATE

STANDARD DRAWINGS

**NORTHERN RIVERS
LOCAL GOVERNMENT**

MANHOLES, FIELD INLET PITS &
GRADED INLET/ JUNCTION PIT DETAILS

STORMWATER STANDARD DRAWING SW-02			
A	B		



Street Blades

Ballina Shire Council - STREET BLADE STANDARD 150mm

Size: Maximum length 1200mm Use increments of 10mm when working out blade lengths
Holes: Yes 2 - 11mm
Material: 6mm Aluminium, Square Cut
Face: White Class 1 Reflective
Overlamine: 1170
Logo: Blue Pantone 3025c Green Pantone 369c
Text: Black 100%

Font: Name 100mm
 AS1744 Series C Narrow.
 For longer names use AS1744 Series B Narrow. For very short names use Series D Narrow.



Abbreviations:

ALY - ALLEY
 AVE - AVENUE
 BVD - BOULEVARD
 CCT - CIRCUIT
 CL - CLOSE
 CT - COURT
 CR - CRESCENT
 DR - DRIVE
 ESP - ESPLANADE
 GR - GROVE
 HWY - HIGHWAY
 JNC - JUNCTION
 LA - LANE
 PDE - PARADE
 PWY - PARKWAY
 PL - PLACE
 PL - PLAZA
 RD - ROAD
 SQ - SQUARE
 ST - STREET
 TCE - TERRACE
 WY - WAY
 WK - WALK



Street Blade Specification Sheet V2.0 - 01.06.2020

7. Items for Traffic Engineering Advice

7. Items for Traffic Engineering Advice

Nil Items

8.1 Schedule of Outstanding Resolutions of the Committee

8. Information for the Committee

8.1 Schedule of Outstanding Resolutions of the Committee

Introduction

List of outstanding resolutions from previous meetings of the Local Traffic Committee.

Information

Meeting Held 13 December 2023

12/23-6.1 Proposed 2H Parking Zone – Ballina Dental Clinic, Fox Street, Ballina

Recommendations

1. Subject to provision of a detailed and compliant signs and pavement marking design, the Committee support provision of three linemarked, 2H timed parking spaces, east of the NSW Health Dental Clinic driveway on the south side of Fox Street, Ballina.
2. The 2H parking be operational Mondays – Fridays from 8am until 4.30pm.
3. That Ballina Shire Council conduct a review of parking on streets surrounding the hospital including both sides of Fox and Cherry Streets, and the hospital side of Moon and Bentinck Streets, with a view to formulating a Master Plan for the precinct.

Action to Date

Referred to Designers

12/23-7.1 Bus Turning Path Issues, Cawley Close, Alstonville

Recommendations

The Committee approved the request to move the “No Stopping” Zone sign to the eastern side of the driveway of 17 Cawley Close, Alstonville.

Action to Date

Referred to Designers

Meeting Held 11 October 2023

10/23-6.1 Proposed No Stopping Zone, Wardell Road, Alstonville

Recommendations

That the Committee supports both Options 2 and 3:

- Option 2 - through use of “No Stopping” yellow line, remove parking on the eastern side of Wardell Road between Daley Street and Coral Street, Alstonville
- Option 3, trial installation of a thermoplastic roundabout at this intersection.

8.1 Schedule of Outstanding Resolutions of the Committee

With a preference for Option 3, subject to detailed design demonstrating the feasibility of this option and available funding.

Action to Date

Referred to Designers

Meeting Held 14 June 2023

06/23-6.2 Proposed Parking Arrangements Tamar Street Ballina, Kerr to Grant Streets

Recommendations

That the Committee support provision of rear to kerb angle parking, at a specific angle to be determined by Council, in Kerr Street between Kerr and Grant Streets, Ballina.

Action to Date

Forwarded to designer.

06/23-8.4 Ballina CBD Parking Study

Recommendations

That the Committee note the Ballina CBD Parking Study and its findings and recommendations.

Action to Date

On public exhibition November 2023, around 180 submissions received. A detailed report on submissions and subsequent recommendations is on the 22 February 2024 Council meeting agenda.

Meeting Held 12 April 2023

04/23-6.4 Request for Accessible Car Park Space at Shaws Bay

Recommendations

That, subject to detailed design in compliance with AS 2890.5, the Committee support provision of an accessible parking space and shared area on the road related area on Lot 5 Section 88 DP 758047 as generally shown on the attachment to this report.

Action to Date

Tasked to designers

04/23-10.1 Request for Accessible Car Parking Space in Perry Street, Alstonville

Recommendations

That the Committee supports the installation of an accessible car parking space in Perry Street, near St Joseph's Primary School, subject to Council providing a plan to Transport for NSW for approval.

Action to Date

Tasked to designer.

Meeting Held 14 December 2022

12/22-6.3 Proposed Parking Space for People with Disabilities and Drop Off/Pick Up Zone, Crane Street, Ballina Adjacent Ballina Primary School

Recommendations

That the Committee support provision of the regulatory signs and markings in Crane Street, Ballina as designated in the attachment to this report.

Action to Date

Pavement markings and signage delayed pending sealing of gravel shoulder.

12/22-7.1 Provision of Accessible Parking, Main Street, Alstonville

Recommendations

That Council:

1. Check the possibility of disabled access parking in Main Street.
2. Investigate proposed disabled parking spaces in the rear carpark either side of the covered walkway.
3. Refer the matter to the Access Committee for further advice prior to resubmitting to the LTC.

Action to Date

1. & 2. Council designers are checking candidate spaces in Daley Street and in public carpark off Commercial Road.
3. Access Committee consulted, but no detailed advice provided.

Meeting with Alstonville Chamber of Commerce

The Road Safety Officer and Traffic Engineer met with representatives of Alstonville Chamber of Commerce on site 10 May 2023. The Chamber will further consider and advise Council of its preferred location for accessible parking. The Chamber will also provide advice on loading zone preferred locations and they requested more compliance action on timed parking. The Rangers have advised that due to staff/resource limitations, the 2H parking is difficult to provide compliance action (parked vehicles need to be monitored for 2 hours +), they advise that if timed parking was changed to 1H, compliance action is likely to be much more effective.

12/22-7.3 Traffic Issues Rifle Range Road, Wollongbar Village

Recommendations

That Council:

1. Consider reconfiguration of the Plateau Drive Roundabout to inhibit high speed drive through on Rifle Range Road.
2. Conduct a behaviour campaign to deter speeding in Wollongbar.
3. Consider provision of centre line and edge line treatment including possible raised pavement markers to confine traffic and inhibit speeding.
4. Install traffic counters in Rifle Range Road to provide data for possible request to TfNSW for a speed zone review.
5. Provide Crash data to assist TfNSW in any speed zone review.

Action to Date

1. Referred to design section for consideration.
2. Facebook action taken by Road Safety Officer.

8.1 Schedule of Outstanding Resolutions of the Committee

3. Additional centreline and edge linemarking to be done when contractors in the area.
4. Traffic count completed 100m east of Midway Ave, 19 January 2023 (see attachment). 85% speed 61.38 kph, median speed 56.62 kph, daily volume 3,136 vpd.
5. Crash data has already provided in report to Committee Delegates.

Meeting Held 10 August 2022

8/22-8.2 Schedule of Outstanding Resolutions of the Committee

Recommendations

1.
2. Item 10/19-6.3 - The Committee recommends a preference for traffic optimisation through the dual lane extension of the eastbound lanes in River Street (Kerr Street to Grant Street) subject to staff collecting more data, public consultation and reporting to Council.

Action to Date

Matter placed on public exhibition. A number of submissions have been received. A report on submissions and subsequent recommendations is planned for the March 2024 Council meeting.

Meeting Held 13 April 2022

4/22-7.2 Four Way Cross Intersections – Ballina Island

Recommendations

The Committee recommended that:

1. Council investigate an option for a centre median in Fox Street.
2. Council consider a road safety message or campaign around give way risks on Ballina Island.

Action to Date

1. Comms team given draft for Community Connect or online Fact Sheet.
2. Design not commenced.

Meeting Held 8 December 2021

12/21-6.2 Proposed Loading Zone – Kalinga Street, West Ballina

Recommendation

That the Committee support provision of a Loading Zone on the north side of Kalinga Street, West Ballina adjacent to No 79, generally as depicted on the above Ardill Payne plan subject to:

1. Submission to and approval by Council of detailed design plans.
2. The owner being responsible for all costs for the Loading Zone and associated works.

Note: Subject to further consultation being undertaken and reviewed with adjoining property owners.

Action to Date

Proponent submitted S138 application, but had insufficient detail and returned to applicant.

Meeting Held 9 December 2020

12/20-7.2 Disability Parking – Ballina CBD Central Block

Recommendation

The Committee approves creation of two additional disability car parking spaces in the Ballina CBD Central Block – one each on both east and west bound lanes.

Action to Date

Design commenced.

12/20-7.3 Shaws Bay Parking – East of Hill Street, East Ballina

Recommendation

The Committee approves creation of 4P area parking on both eastern and western sides of Hill Street, in Hill Street, Brighton Street, Park Street, The Serpentine and surrounding grassed areas of Shaws Bay recreation area.

Action to Date

Installation not commenced, pending further public consultation.

Meeting Held 12 August 2020

8/20-6.4 Proposed Additional Disabled Parking Space - Off Isabella Drive, Skennars Head

Committee Recommendation

That subject to funding arrangements being provided to the satisfaction of Council, the Committee support provision of an additional disabled car parking space adjacent to the existing disabled car parking space in front of Holy Family School in the road related area off Isabella Drive, Skennars Head.

Action to Date

Awaiting response from school.

Meeting Held 12 June 2019

2/19-6.5 St Francis Xavier Catholic Primary School, Proposed Student Drop Off / Pick Up Zone - Martin Street, Ballina

Recommendation

1. The Committee support a NO PARKING (8–9:30 am and 2:30–4pm School Days) student drop off/pick up zone in Martin Street, Ballina adjacent to St Francis Xavier Catholic Primary School as depicted on the attachment to this report.
2. Implementation of the zone to be deferred pending provision of funding by the school for necessary road shoulder pavement and sealing works, provision and erection of associated signage.

Action to Date

Implementation deferred pending funding and completion of associated construction works.

RECOMMENDATIONS

That the Committee note the information in the report regarding the Schedule of Outstanding Resolutions.

Attachment(s)

Nil

9. Regulatory Matters on Classified Roads

9. Regulatory Matters on Classified Roads (GM's Delegate)

Nil Items

- 10 Items Without Notice
 - 11 Next Meeting
-

10. Items Without Notice

11. Next Meeting

Next meeting is scheduled for Wednesday 10 April 2024 at 10:00 am.